LEGEND OF SYMBOLS & ABBREVIATIONS:

EXISTING:		PROPOSED:
	PROPERTY LINE	
	BORDERING VEGETATED WETLAND	N/A
	BUFFER ZONE TO RESOURCE AREA	N/A
— — —102— — —	INTERMEDIATE CONTOUR	102
<u> </u>	INDEX CONTOUR	110
EP	EDGE OF PAVEMENT	EP
BB	BITUMINOUS BERM	BB
VGC	VERTICAL GRANITE CURB	VGC
SGC	SLOPED GRANITE CURB	SGC
CC	CONCRETE CURB	cc
-00	GUARD RAIL	-0
D	DRAIN	D
s	SEWER	s
w w	WATER	w w
	LINDERGROUND ELECTRIC	

 — T
 TELEPHONE
 — T

 — G
 NATURAL GAS
 — G

 — X
 CHAIN LINK FENCE
 — X

 — WOOD FENCE
 — — —

 — — SILT FENCE
 — — —

 — HAY BALES
 — — — —

 TREE LINE
 — — — — —

 RETAINING WALL
 — — — — —

TREE LINE

RETAINING WALL

STONE WALL

STONE BOUND

CB(SET)

CONCRETE BOUND

IP(SET)

IRON PIPE

OH(SET)

DRILL HOLE

#A-11

WETLAND FLAG

X

SPOT ELEVATION

CATCH BASIN

DRY WELL

DRAIN MANHOLE

SEWER MANHOLE

ELECTRIC MANHOLE

UTILITY MANHOLE

FIRE HYDRANT

GATE VALVE

LIGHT

UTILITY POLE

GUY WIRE

MONITORING WELL

TEST PIT

PERCOLATION TEST

BENCH MARK

TRAFFIC FLOW DIRECTION

DRAINAGE FLOW DIRECTION

LIST OF ABUTTING PROPERTIES

#14 BOYD DRIVE / B. WOOD & SOARS

#14 BRIGGS ROAD / K. CHAMBERS

#3 BROWN AVE. / S. ASPROGIANNIS

#13 LAUREL ROAD / A. & D. MCCARTHY

FERRY ROAD / CITY OF NEWBURYPORT

#17 BRIGGS ROAD / J. BELL & L. MATTHEWS

#7 BROWN AVE. / W. MARGARET & W. BOOK

INTERSTATE 95 / COMMONWEALTH OF MASSACHUSETTS

WOODMAN WAY / VILLAGE AT NEWBURYPORT CONTINUUM

#12 WOODMAN WAY / ADVANCE INVESTMENT PROPERTIES

#23 BOYD DRIVE / S. BLANCHETTE & J. RIVERA-BLANCHETTE

#12 BOYD DRIVE / R. BAILLY

#5 BROWN AVE. / D. COX

#9 BROWN AVE. / P. CUSACK

#16 LAUREL ROAD / E. KOVACH

N/A

ZONING MATRIX: PER OSRD SPECIAL PERMIT

OWNER REFERENCES

18 BOYD DRIVE "EVERGREEN GOLF COURSE"

DEED REFERENCE: BOOK 29288 PAGE 376

DEED REFERENCE: BOOK 5304 PAGE 173

ASSESSORS: MAP 110 PARCEL 20

ASSESSORS: MAP 111 PARCEL 13

5 BROWN AVENUE

OWNER: DORRIS COX

REQUIRED: R-1 ZONE

LOT AREA 10,000 S.F.

BUILDING HEIGHT <35'

MIN. FRONT SETBACK= 15' MIN.

SIDE SETBACK= 10' MIN.

REAR SETBACK= 10' MIN.

OPEN SPACE RESIDENTIAL DEFINITIVE SUBDIVISION OF LAND & W.R.P.D. SPECIAL PERMIT NEWBURYPORT, MASSACHUSETTS

PREPARED FOR:
EVERGREEN COMMONS LLC
487 GROTON ROAD, SUITE A
WESTFORD, MASSACHUSETTS



LOCUS PLAN
SCALE: 1"=500'±

SHEET INDEX (CONT'D):

OFF	SHELT INDEX.			
SHEET NO.	DESCRIPTION			
T1	COVER SHEET			
EX1	EXISTING CONDITIONS PLAN OF LAND (1 OF 6)			
EX2	EXISTING CONDITIONS PLAN OF LAND (2 OF 6)			
EX3	EXISTING CONDITIONS PLAN OF LAND (3 OF 6)			
EX4	EXISTING CONDITIONS PLAN OF LAND (4 OF 6)			
EX5	EXISTING CONDITIONS PLAN OF LAND (5 OF 6)			
EX6	EXISTING CONDITIONS PLAN OF LAND (6 OF 6)			
S1	LOT LAYOUT KEY SHEET			
S2	LOT LAYOUT SHEET 1			
S3	LOT LAYOUT SHEET 2			
S4	LOT LAYOUT SHEET 3			
S5	LOT LAYOUT SHEET 4			
S6	LOT LAYOUT SHEET 5			

SHEET INDEX:

SHEET NO.

C1 CIVIL KEY SHEET

C2 GRADING PLAN 1

C3 GRADING PLAN 2

C4 GRADING PLAN 3

C5 GRADING PLAN 4

C6 DRAINAGE & UTILITY PLAN 1

C7 DRAINAGE & UTILITY PLAN 2

C8 DRAINAGE & UTILITY PLAN 3

C9 DRAINAGE & UTILITY PLAN 4

C10 STREET PLAN & PROFILE ROAD "A" & "D"

C11 STREET PLAN & PROFILE ROAD "B"

C12 STREET PLAN & PROFILE ROAD "C"

C13 TYPICAL SECTIONS, DETAILS & NOTES 1

TYPICAL SECTIONS, DETAILS & NOTES 2

TYPICAL SECTIONS, DETAILS & NOTES 3

TYPICAL SECTIONS, DETAILS & NOTES 4

BENCHMARK:

SHEET INDEX (CONT'D):

DESCRIPTION

LANDSCAPE PLAN

LANDSCAPE PLAN

LANDSCAPE PLAN

LANDSCAPE PLAN

OVERALL LANDSCAPE PLAN

ARCHITECTURAL ELEVATIONS

THE GRADES SHOWN ARE ON NAD88 VERTICAL DATUM ESTABLISHED BY RTK GPS. HYDRANT BONNET NUT LOCATED AT THE END OF BOYD DRIVE IS ELEVATION 63.05

CUT & FILL OPERATIONAL STATEMENT

APPROXIMATELY 8,500 CUBIC YARDS OF LOAM WILL BE STRIPPED AND REMOVED FROM THE SITE FOR THE CONSTRUCTION OF THE ROADWAYS. THE ROADWAYS WILL REQUIRE APPROXIMATELY 18,000 CUBIC YARD OF FILL FOR SUBGRADE MATERIAL. FROM THE EXCAVATION FOR THE CONSTRUCTED STORMWATER WETLANDS AND SITE GRADING WE EXPECT TO HAVE 70,000 CUBIC YARDS OF SUITABLE MATERIAL FOR THIS USE THAT WILL BE USED FOR FILLING IN THE PROPOSED DEVELOPMENT AREA. THE PROJECT AS DESIGN IS BALANCED EXCEPT FOR REQUIRED ROADWAY SUBGRADE BASE AND UTILITY BACKFILL BORROW MATERIAL.

FOR REGISTRY OF DEEDS USE ONLY

SCENIC ROAD:

THE EXISTING STREETS ARE NOT SUBJECT TO THE SCENIC ROAD ACT.

WATER DISTRICT:

THE SUBJECT PROPERTY RESIDES IN THE CITY OF NEWBURYPORT WATER

THE SUBJECT PROPERTY RESIDES IN ZONE II PUBLIC WATER SUPPLY PROTECTION AREAS ARE DEFINED IN THE DRINKING WATER REGULATIONS AT 310 CMR 22.0. THE PROPERTY ALSO RESIDES IN THE NEWBURYPORT WATER RESOURCE PROTECTION DISTRICT ZONE II.

ROADWAY CONSTRUCTION WAIVERS

 $5.4.2.\mathrm{D}$ NAVD 88 HAS BEEN USED INSTEAD OF NGVD 29 SINCE THE CITY GIS BASE MAPPING IS ON NAVD 88 DATUM.

5.4.2.F LOT WIDTH IS NO LONGER APPLICABLE TO ANY LOTS. SQUARE OF REQUIRED 80% IS SHOWN IN THREE LOTS ON BOYD DRIVE AS NECESSARY. REMAINING LOTS ARE NOT SUBJECT TO SAID REQUIREMENT.

5.4.2.K STONE WALLS, FENCES, CART PATHS, WATER BODIES AND WATER COURSES WITHIN THE BOUNDARIES OF THE PROPERTY ARE NOT SHOWN ON LOT LAYOUT PLAN AS THEY ARE INTRINSIC TO THE OPERATION OF THE GOLF COURSE AND NOT BELIEVED TO BE EVIDENCE OF ADVERSE USE OR OCCUPATION. THERE ARE NO NATURAL WATER COURSES OR WATER BODIES ON THE PROPERTY. SAID FEATURES MAY BE FOUND ON THE EXISTING CONDITIONS PLAN.

6.12.1 MONUMENTS ARE PROPOSED AT STREET LINE INTERSECTIONS WITH EXISTING ROADS, AT CHANGES IN CURVATURE OR DIRECTION OF THE ROAD. NO LOT LINE ON A PROPOSED OR EXISTING STREET WILL BE MORE THAN APPROXIMATELY 100' FROM ANY NEWLY INSTALLED MONUMENT. NO ADDITIONAL MONUMENTS ARE PROPOSED ON

6.8 RIGHT OF WAY - MIN. RIGHT OF WAY WIDTH 50' REQUIREMENT, 40' PROPOSED.

6.8 MINIMUM ROADWAY CENTERLINE CURVE RADIUS OF 225', 125' RADIUS IS PROPOSED.

6.8.3 MINIMUM PAVEMENT WIDTH OF 24' REQUIRED, 2S' PAVEMENT WIDTH PROPOSED.

6.9 CURBING - 6" VERTICAL GRANITE REQUIRED, 5" VERTICAL GRANITE CURB PROPOSED.6.14.11- DRAINAGE EASEMENTS, 30' REQUIRED, 15' PROPOSED FOR PIPING TO

BIO-INFILTRATION AREAS AND 20' FOR WATER LINE.

I CERTIFY THAT THE SURVEY PERFORMED TO PREPARE THIS PLAN CONFORMED TO THE

PROCEDURAL AND TECHNICAL STANDARDS FOR AN ON-THE-GROUND CADASTRAL SURVEY IN ACCORDANCE WITH COMMONWEALTH OF MASSACHUSETTS 250 CMR SECTION 6.0.1 AND THAT THE CERTIFICATION SHOWN HEREON IS INTENDED TO MEET REGISTRY OF DEEDS REQUIREMENTS AND IS NOT A CERTIFICATION TO THE TITLE OR OWNERSHIP OF THE PROPERTY SHOWN.

DATE: JAN. 7, 2018

CLERK

P.E. STEPHEN B. SAWYER, PE MASS. REGISTRATION NO. 38800

DATE: JAN. 7, 2018

CITY OF NEWBURYPORT NEWBURYPORT PLANNING BOARD
OFFICE OF THE TOWN CLERK

I, CLERK OF THE CITY OF NEWBURYPORT,
HEREBY CERTIFY THAT THE NOTICE OF
APPROVAL OF THIS PLAN BY THE
PLANNING BOARD HAS BEEN RECEIVED
AND RECORDED AT THIS OFFICE AND NO
NOTICE OF APPEAL WAS RECEIVED
DURING THE TWENTY DAYS NEXT AFTER
SUCH RECEIPT AND RECORDING OF SAID
NOTICE.

DATE

DR BY: WA
CHK BY: S
PROJ NO:

SHEET NO:

DR BY: WAK

CHK BY: SBS

PROJ NO: 2015-063

DATE: June 2, 2017

SCALE: N.T.S.

Winter GEC, LLC

34 WINTER STREET
NEWBURYPORT, MA 01950
978-270-8626

PROJECT TEAM

18 BOYD DRIVE,
SUBDIVISION
NEWBURYPORT, MA
PREPARED FOR
EVERGREEN
COMMONS, LLC

PROJECT INFO

3. CON.COM. PLANS 1/09/18
2. REVIEW COMMENTS 11/16/17

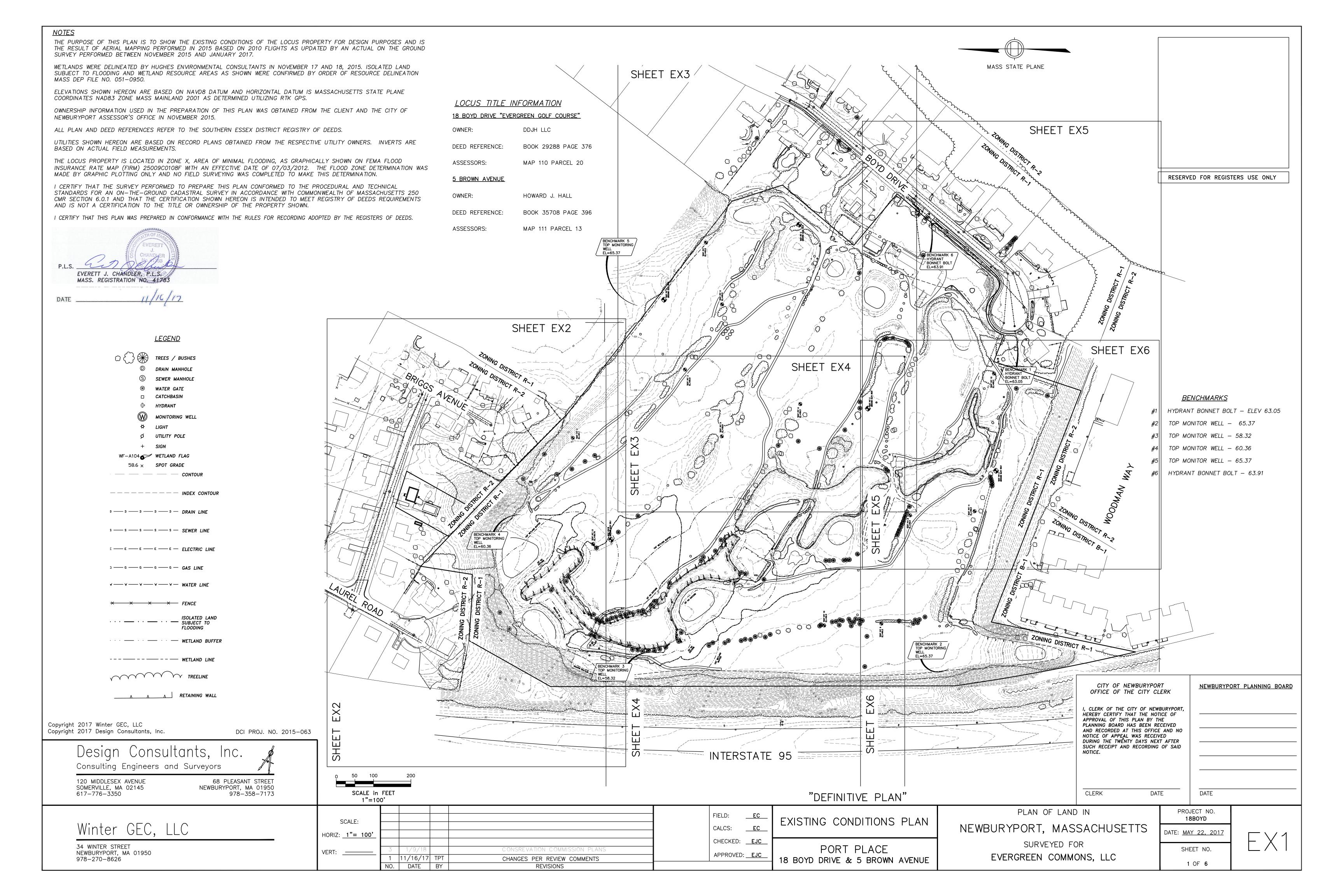
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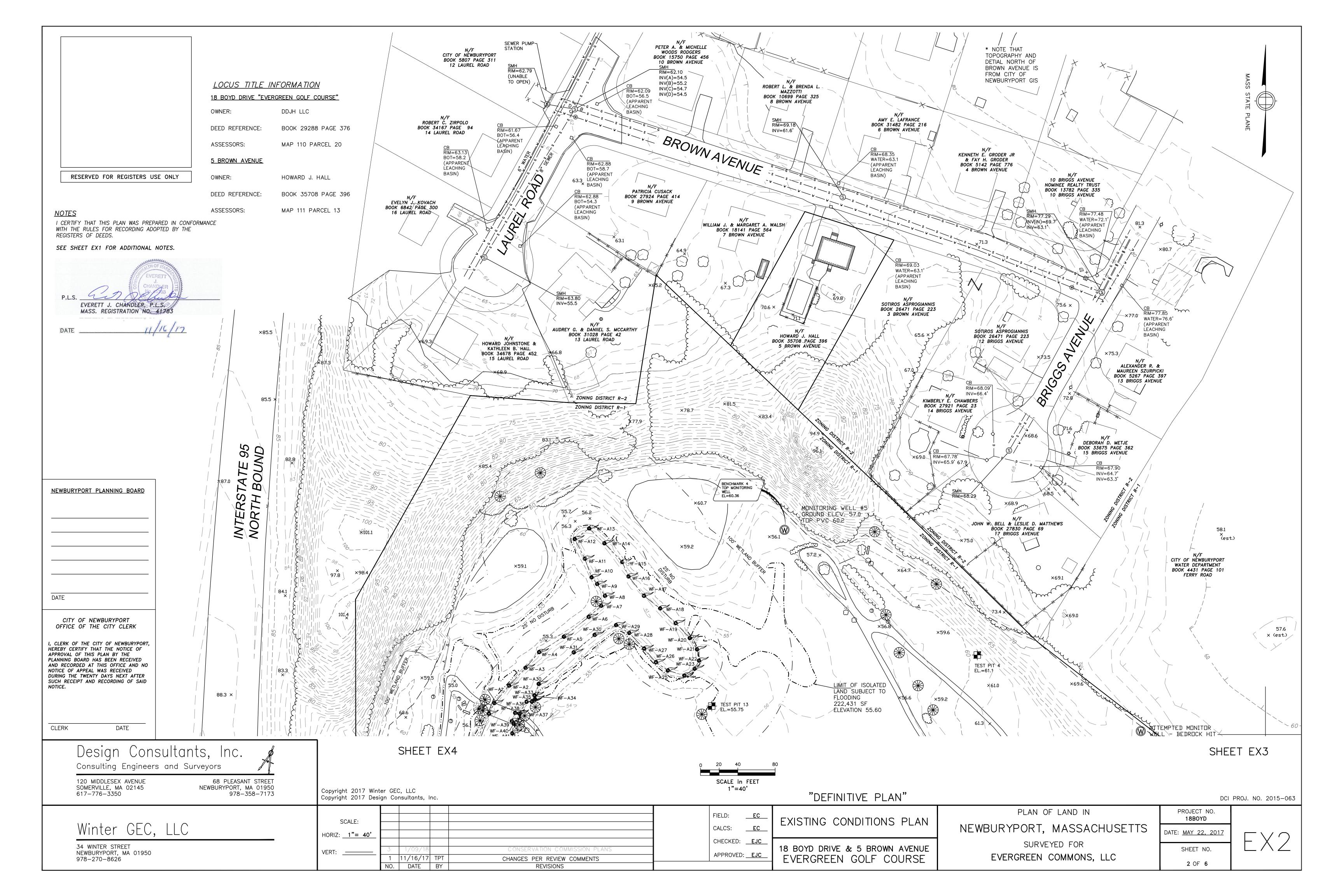
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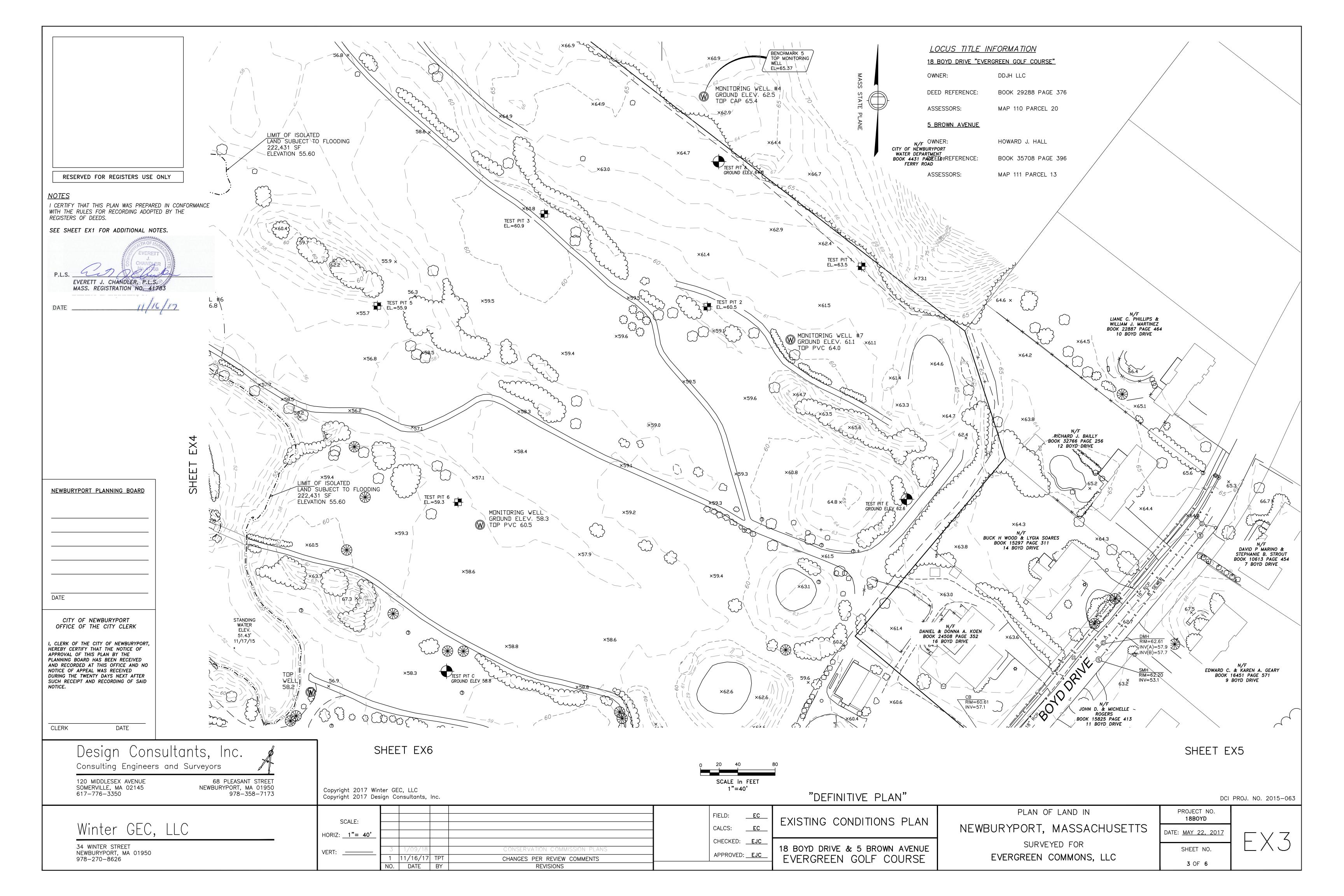
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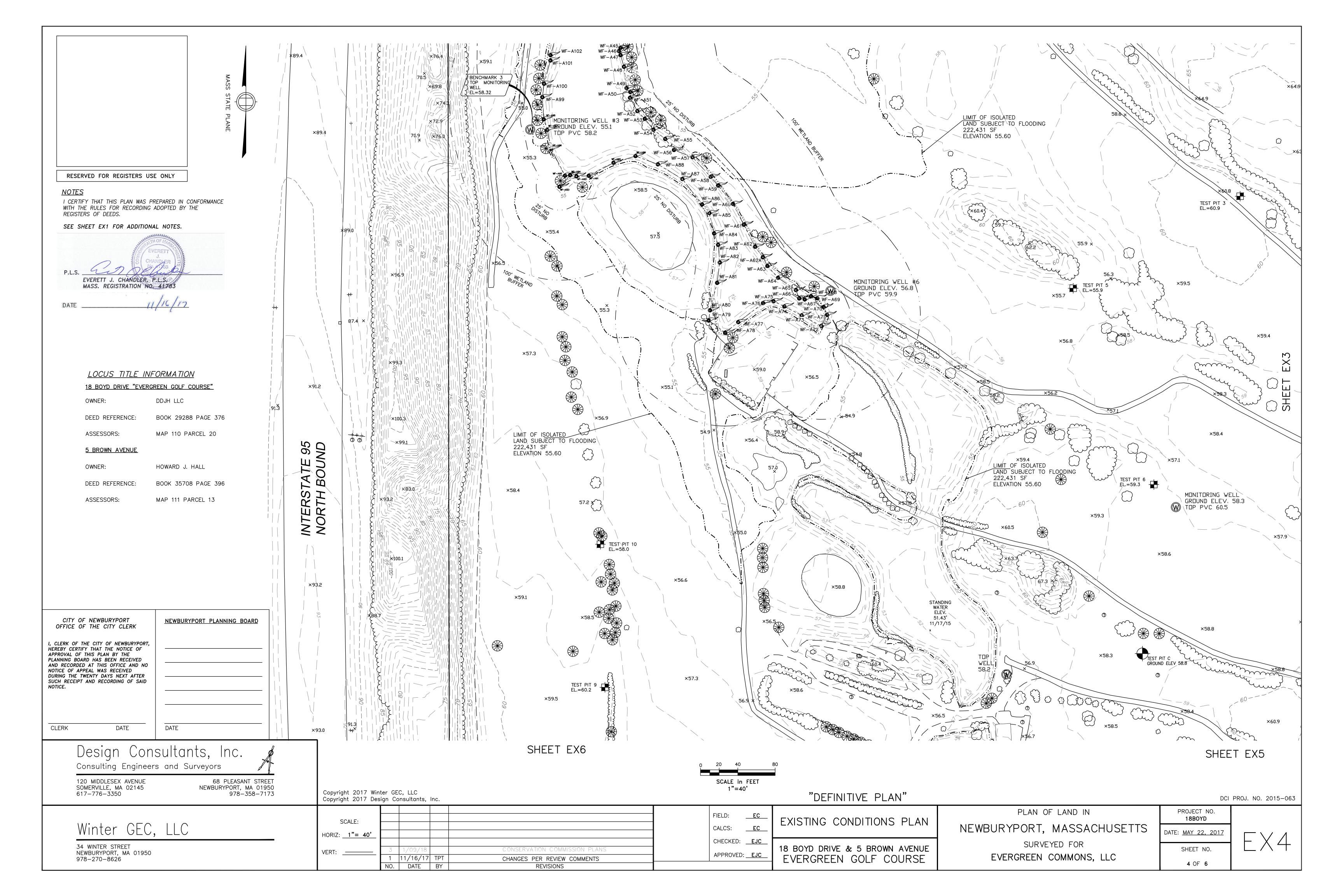
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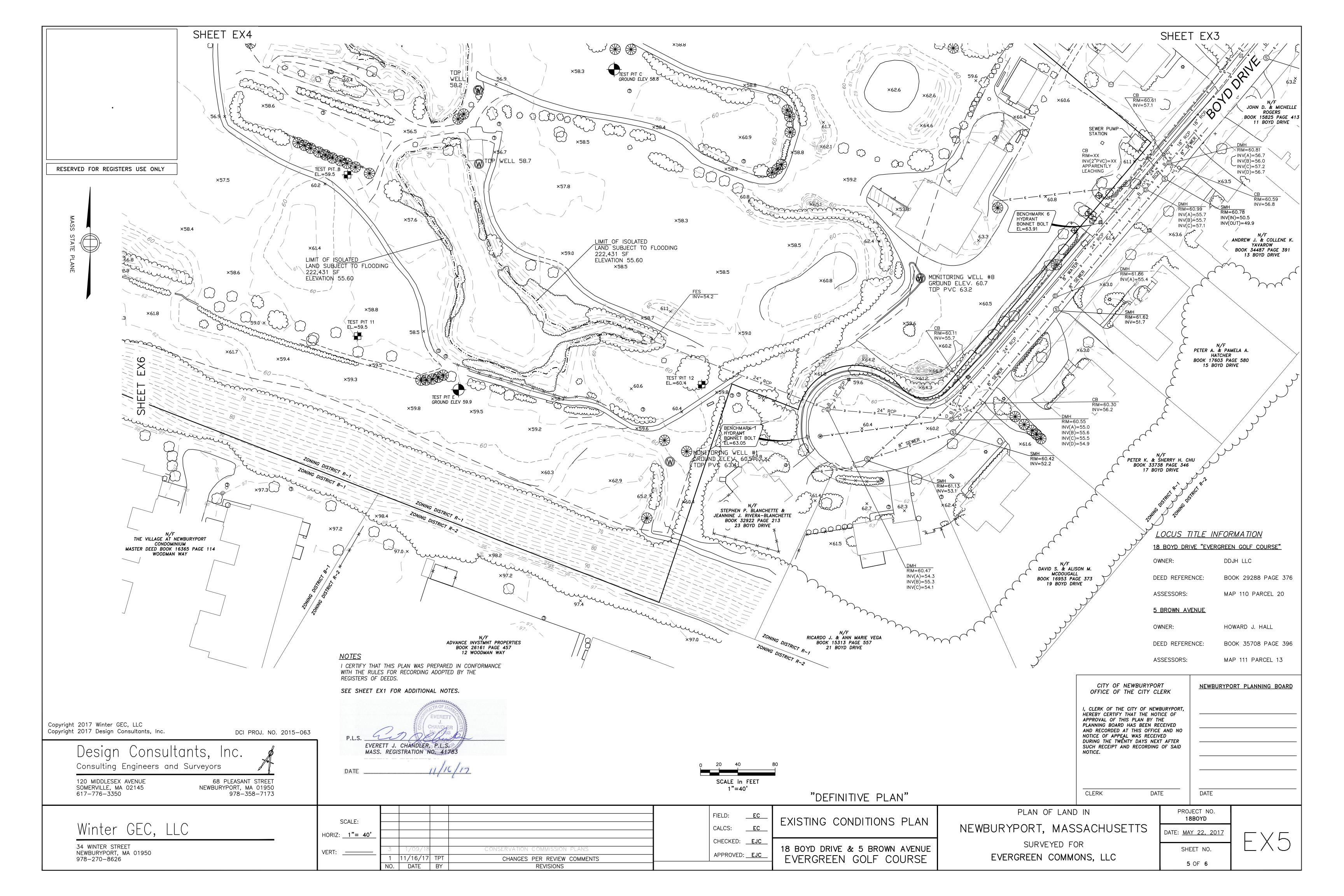
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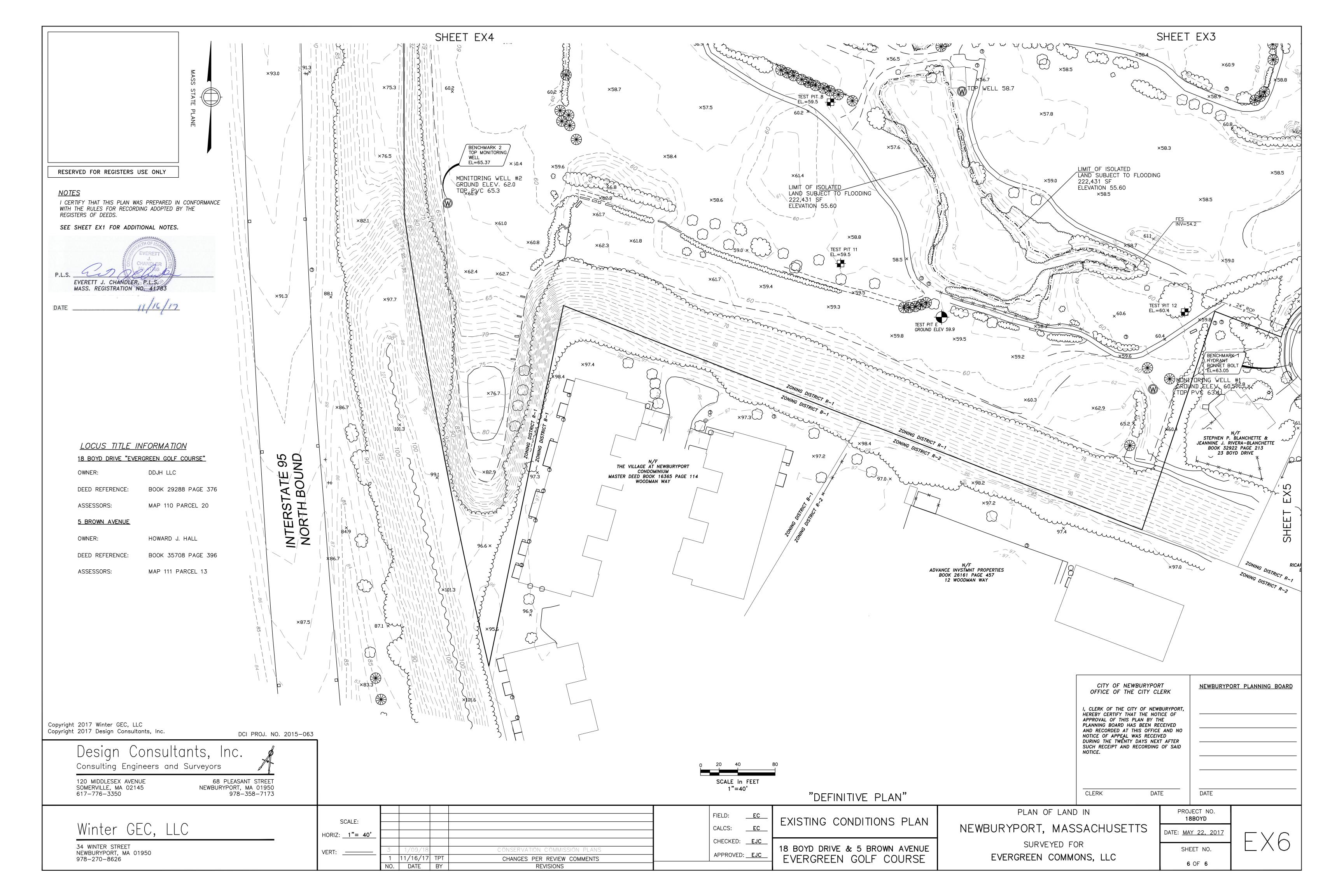


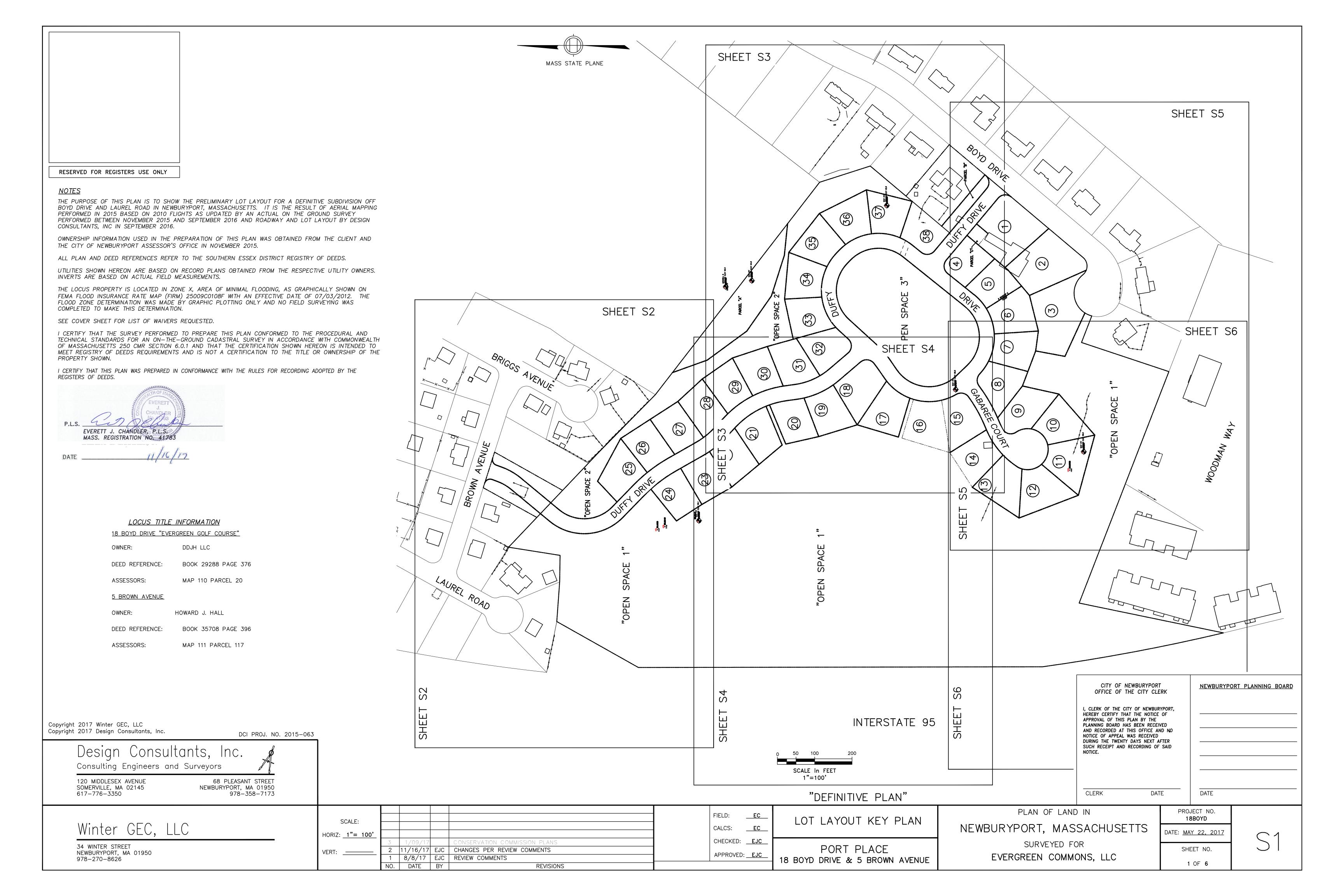


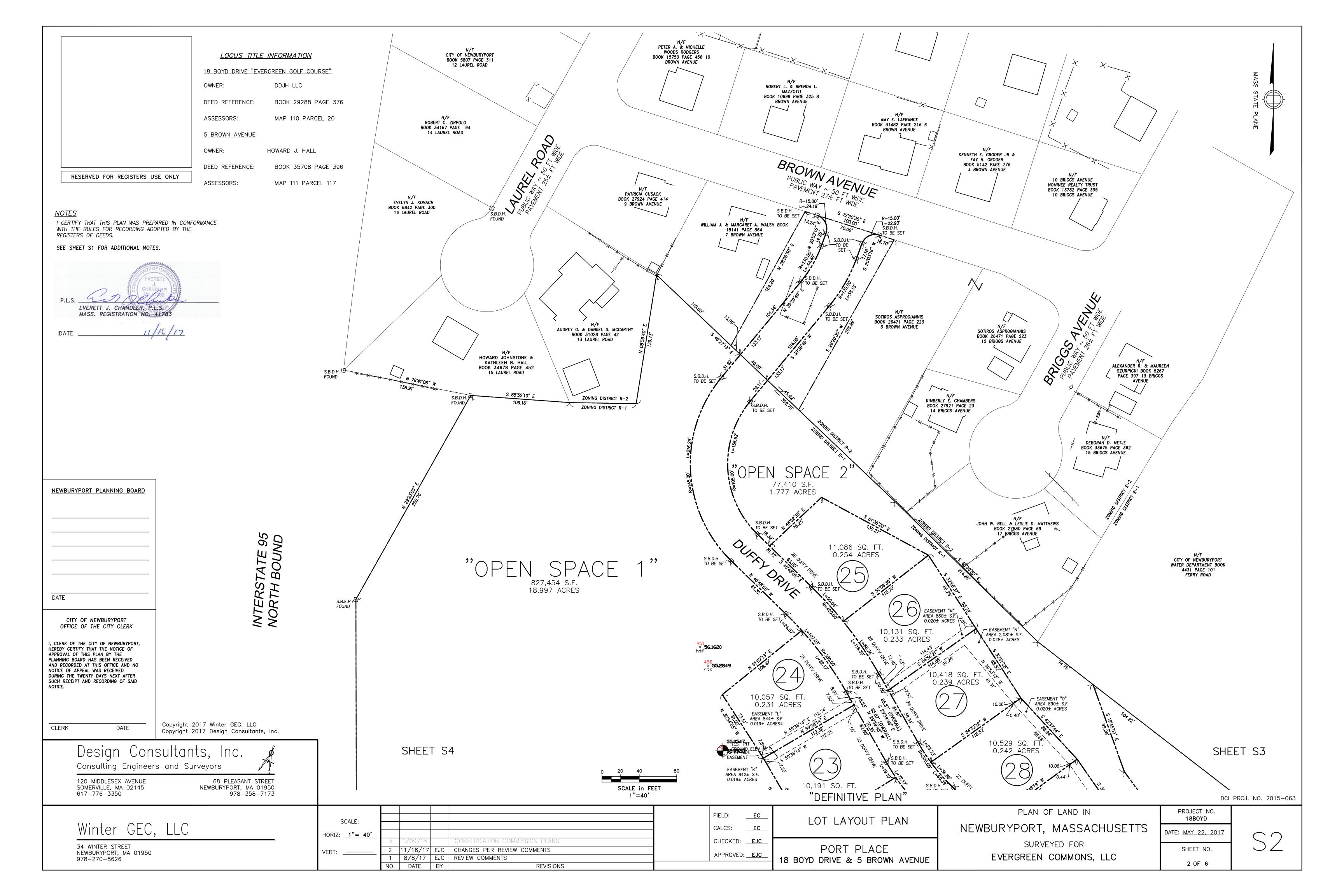


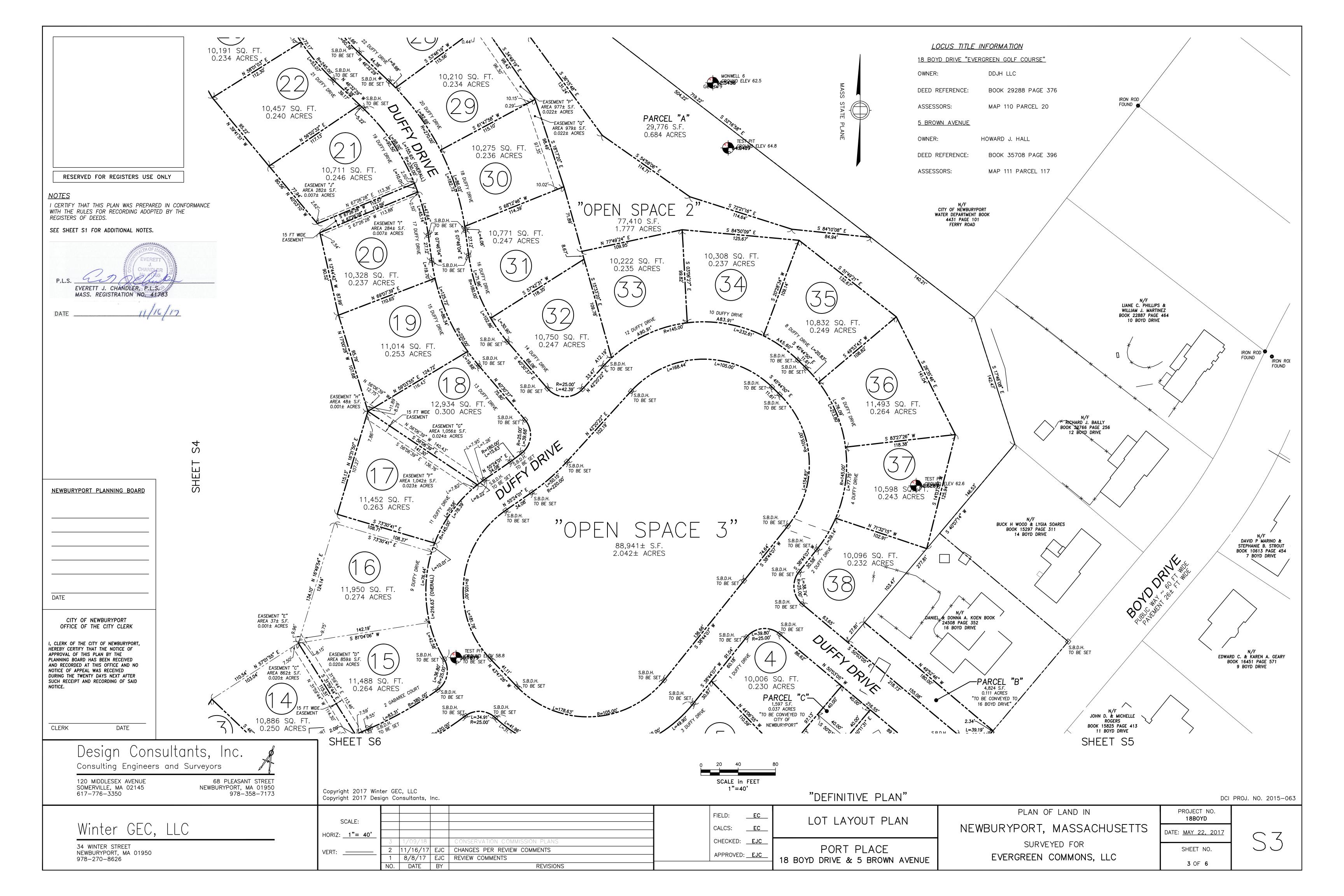


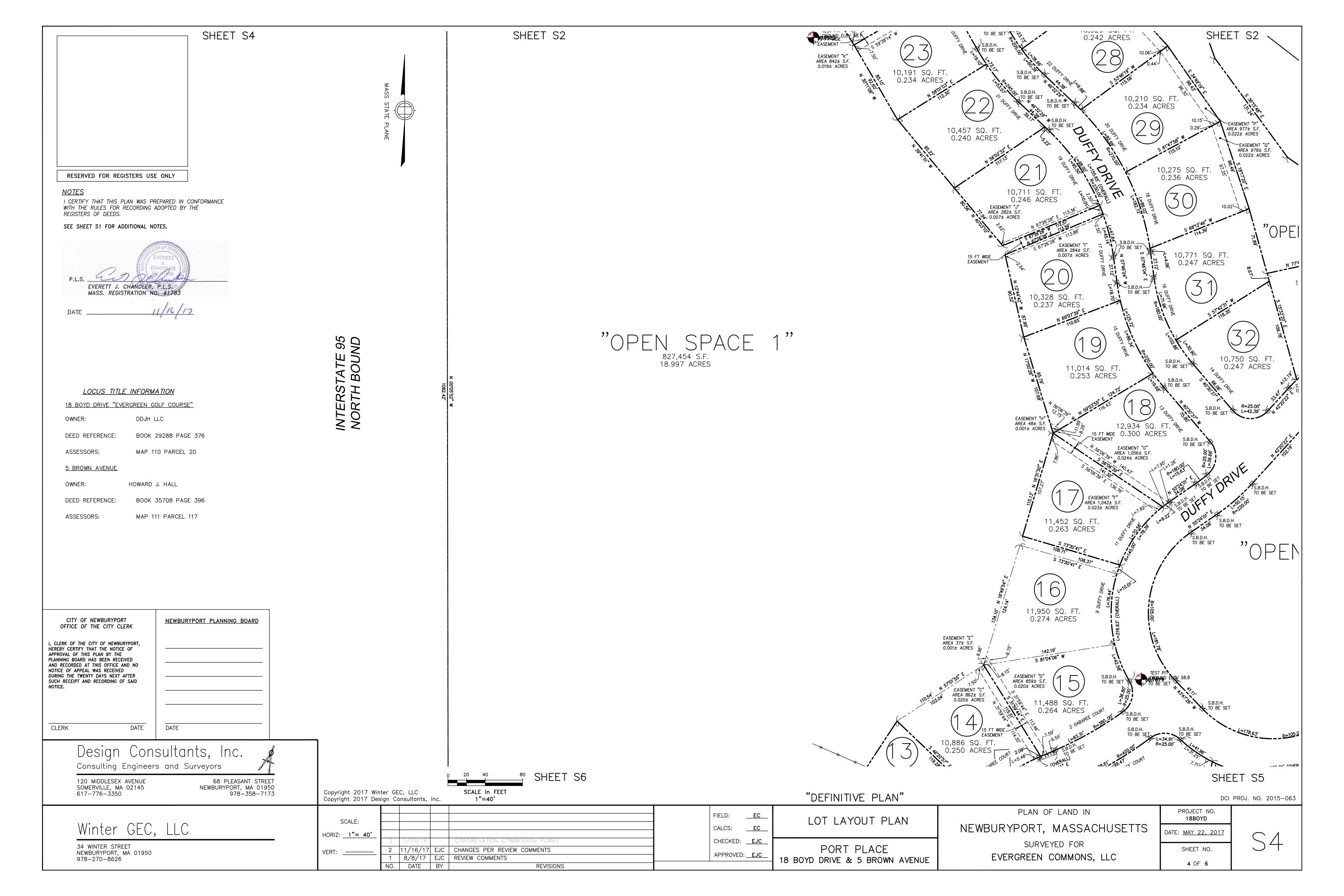


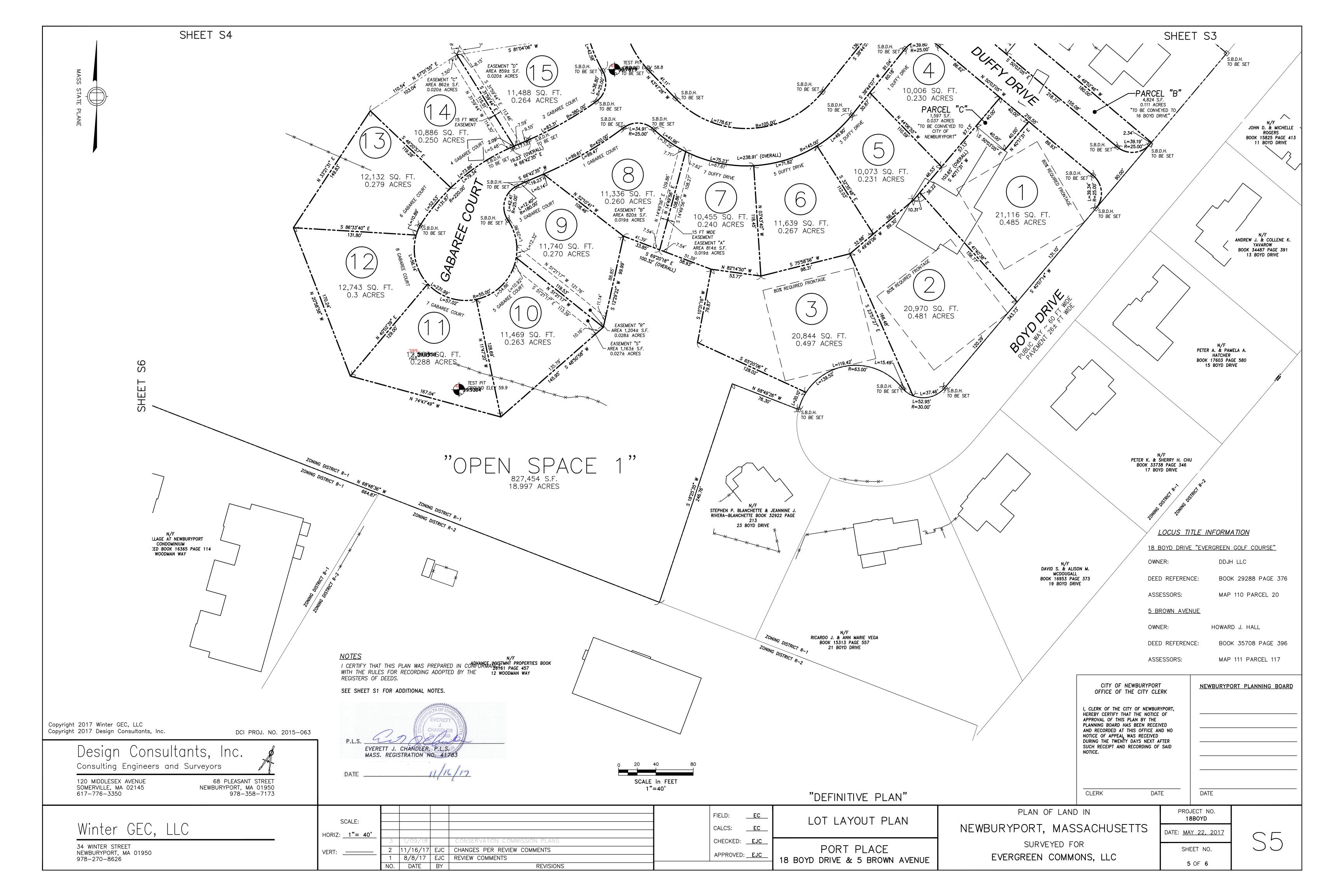


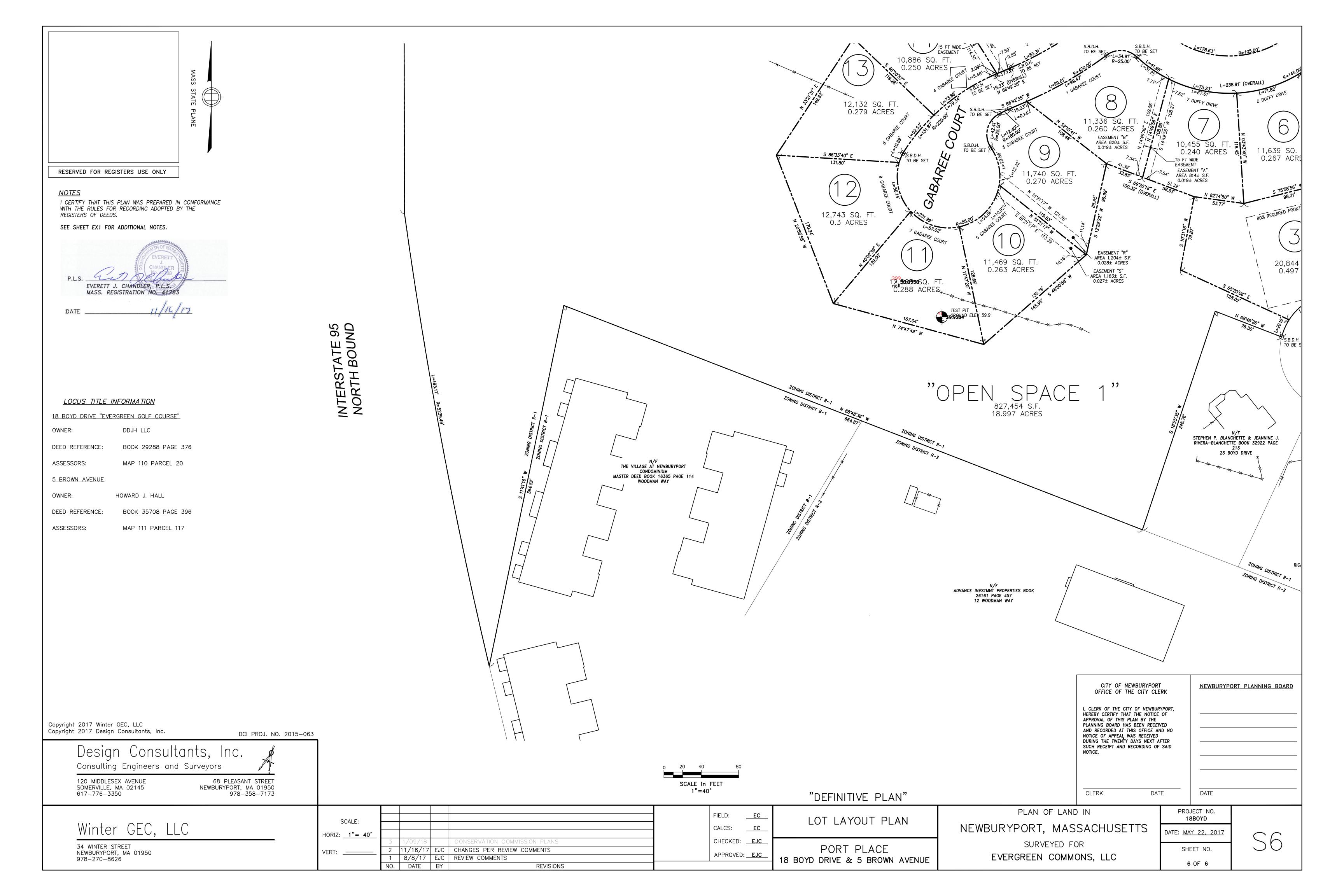


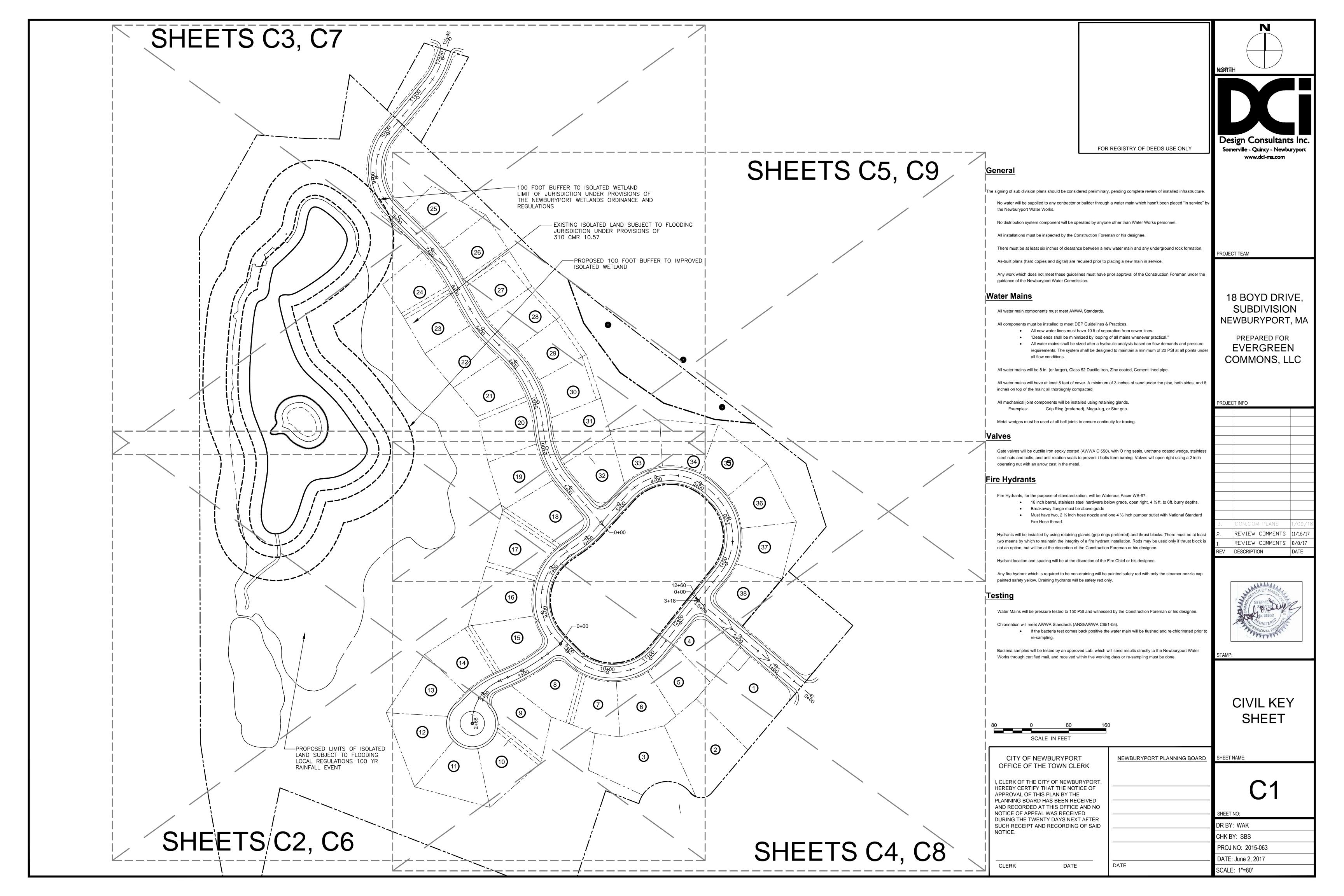


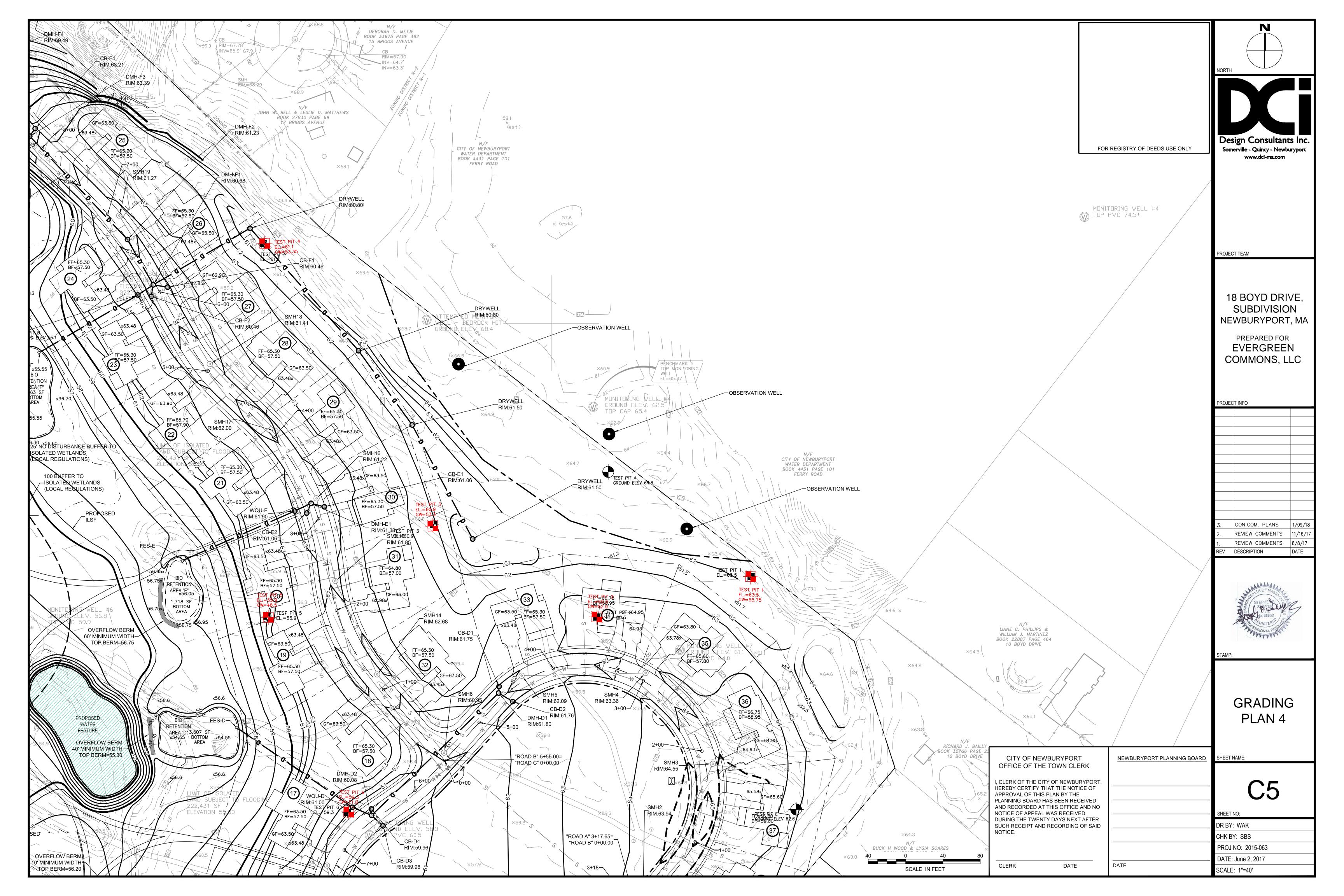


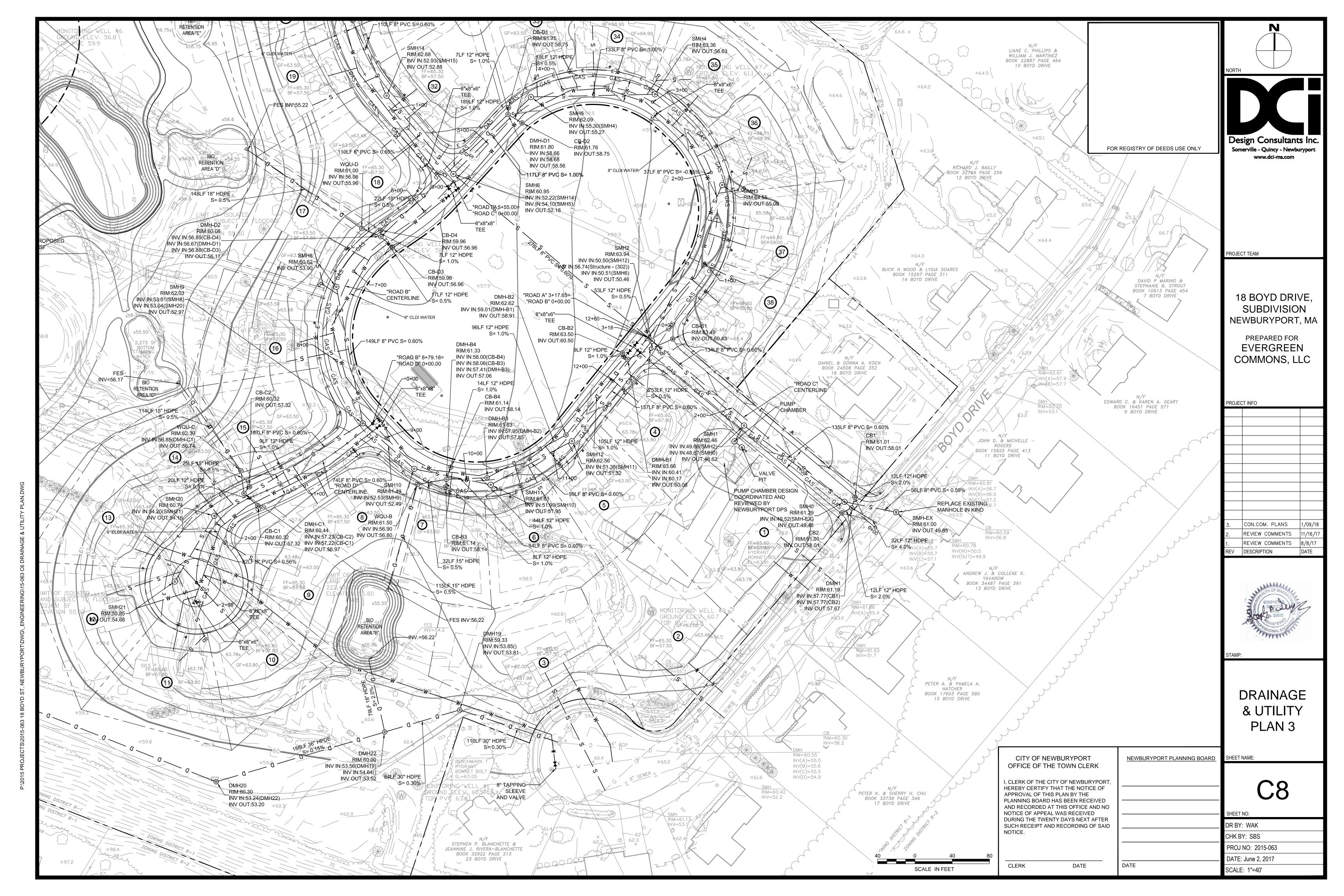


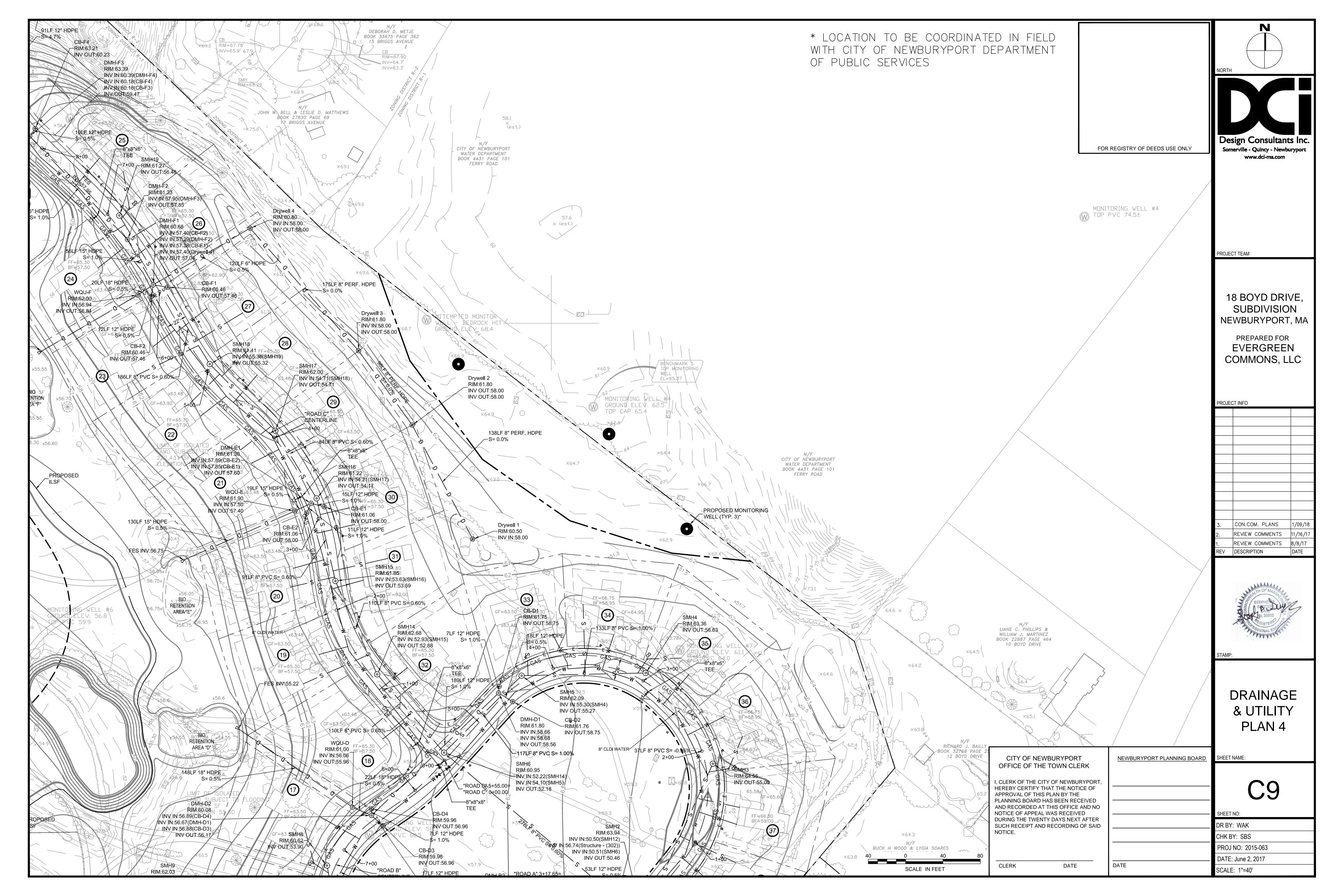


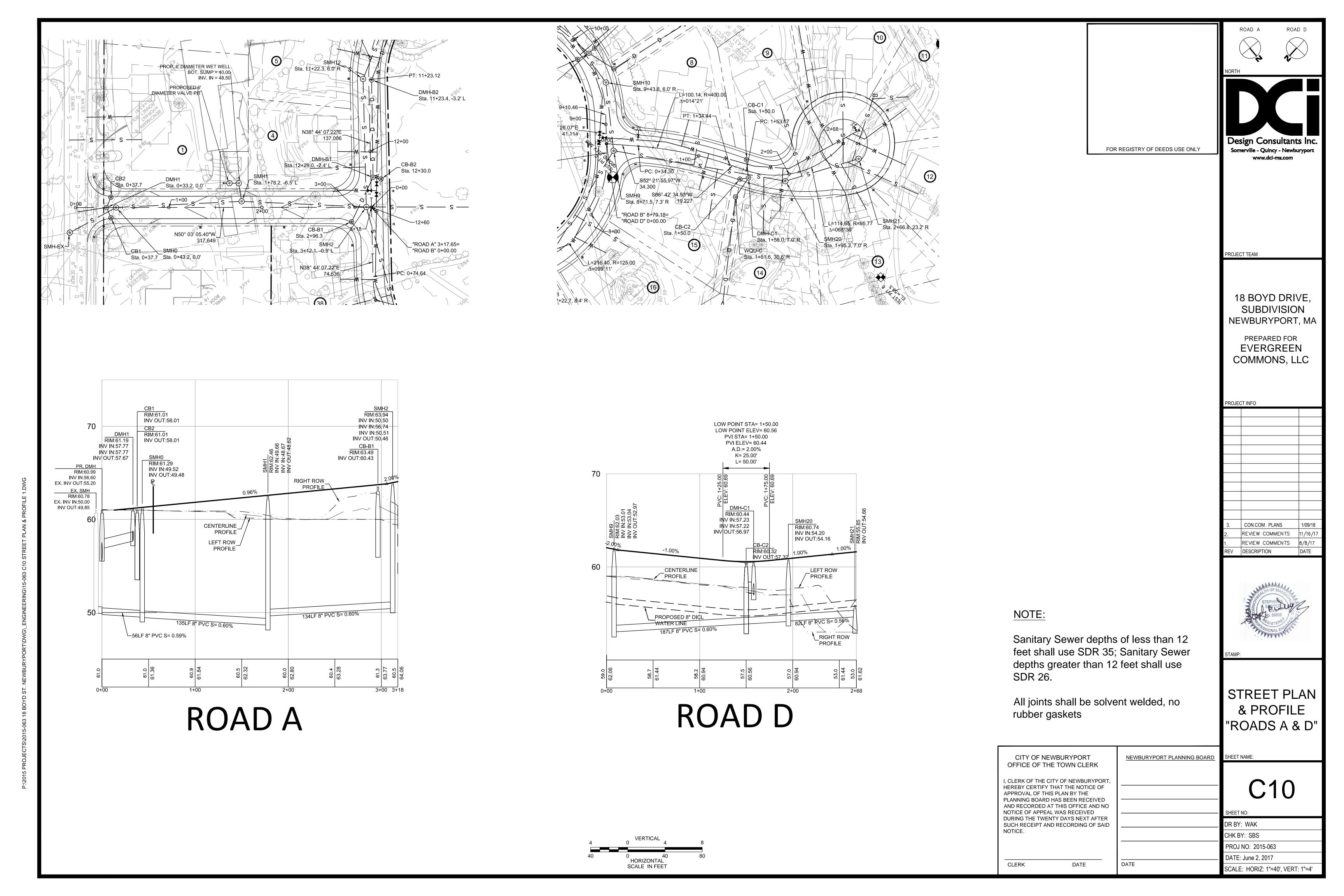


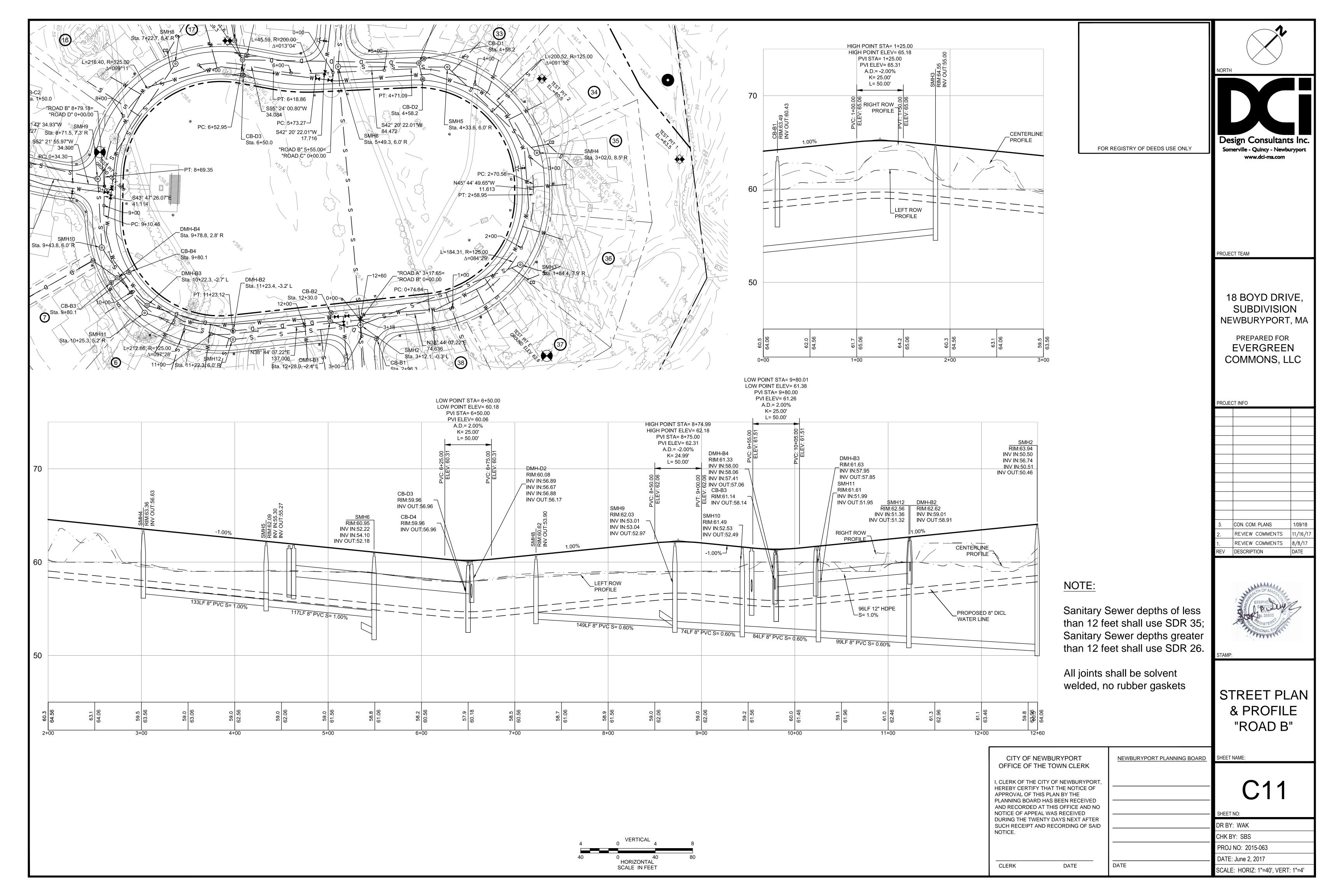


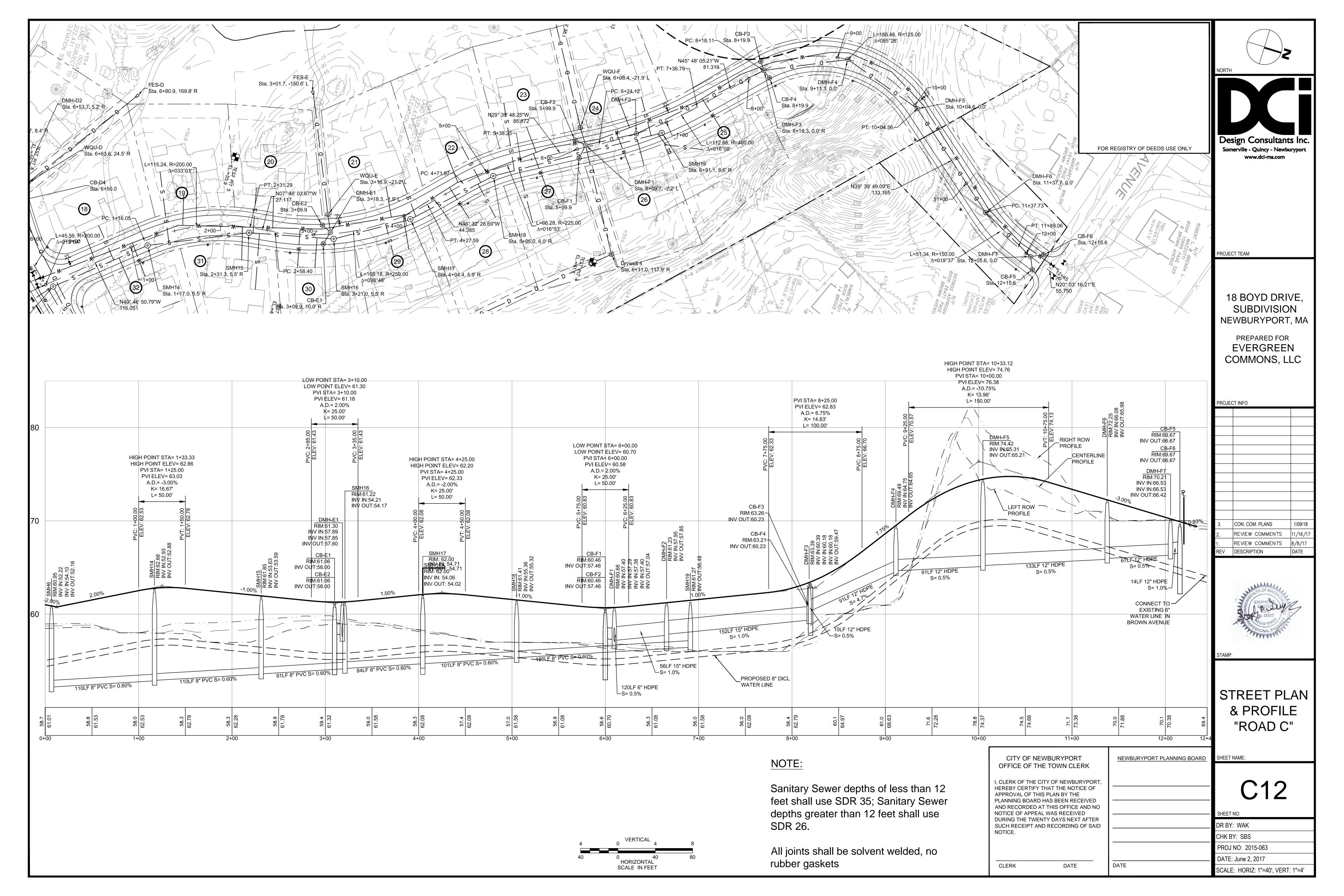


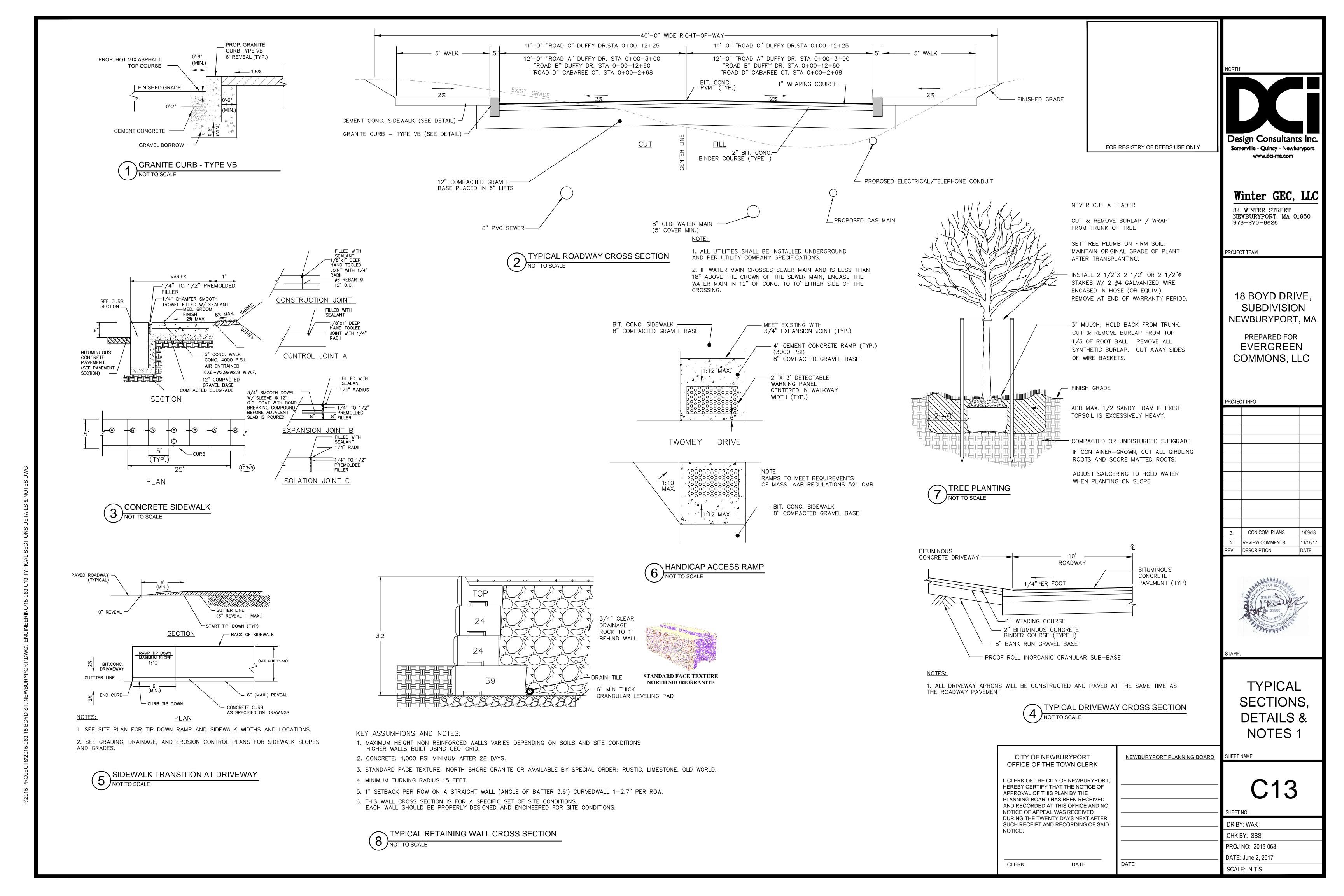


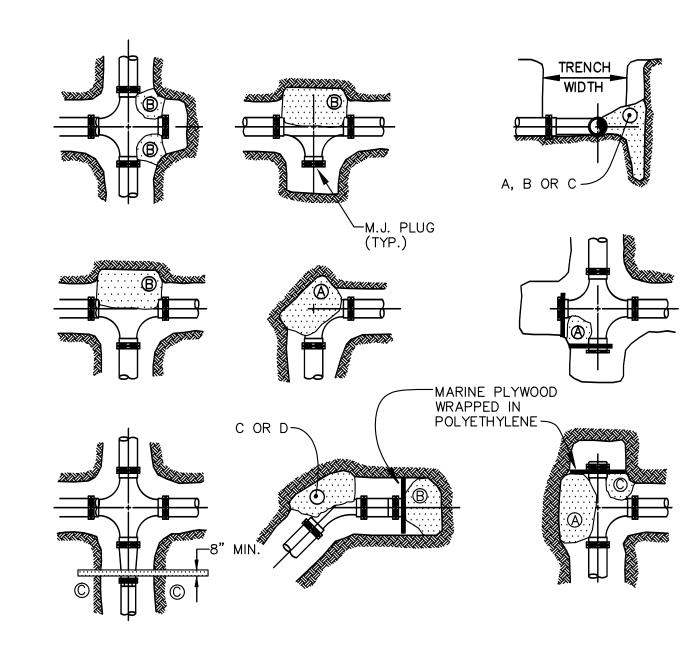


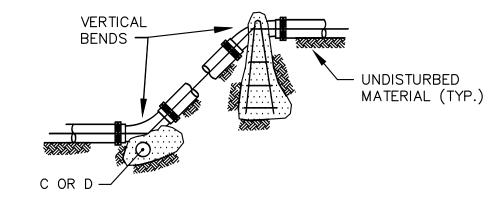












200psi	SQUARE FEET OF CONCRETE THRUST BLOCKING BEARING ON UNDISTURBED MATERIAL						
= 2(CTION	PIPE SIZE				
	T\	/PE	4"	6"	8"	10"	12"
EST PRESSURE	B 18 C 4	0° 80° 5° 2–1/2° 1–1/4°	0.89 0.65 0.48 0.25 0.13	2.19 1.55 1.19 0.60 0.30	3.82 2.78 2.12 1.06 0.54	11.14 8.38 6.02 3.08 1.54	17.24 12.00 9.32 4.74 2.38

1. POUR THRUST BLOCKS AGAINST UNDISTURBED MATERIAL, WHERE TRENCH WALL HAS

MATERIAL. NO JOINTS SHALL BE COVERED WITH CONCRETE

2. ON BENDS AND TEES, EXTEND THRUST BLOCKS FULL LENGTH OF FITTING.

BEEN DISTURBED, EXCAVATE LOOSE MATERIAL AND EXTEND THRUST BLOCK TO UNDISTURBED

FINISHED GRADE

4'-0" SUMP MIN.

- MAGNETIC CAUTION TAPE

1. FINAL SIZE, LOCATION AND NUMBER OF CONDUITS SHALL BE DETERMINED BY THE

CITY OF NEWBURYPORT

OFFICE OF THE TOWN CLERK

I, CLERK OF THE CITY OF NEWBURYPORT.

HEREBY CERTIFY THAT THE NOTICE OF

APPROVAL OF THIS PLAN BY THE

NOTICE.

CLERK

2. ELECTRIC CONDUIT UNDER PAVEMENT AREAS SHALL BE GALVANIZED STEEL OR

TYPICAL ELECTRICAL & COMMUNICATIONS TRENCH

-CONSTRUCT FULL DEPTH PAVEMENT

MATERIAL WITH LOAM AND SEED IN PERVIOUS AREAS AS SPECIFIED

24" MIN.

SECTION OR USE SUITABLE BACKFILL

PRECAST CONCRETE CATCH BASIN

- CONCRETE COLLAR

- MASTIC GASKET

4000 PSI AT 28 DAYS

 PRECAST CONCRETE TO CONFORM TO ASTM C-478 CONC. OF

GRAVEL BEDDING

MATERIAL ALL MANHOLE JOINTS

6" SCREENED

MUNICIPAL STANDARD CATCH

BASIN FRAME AND GRATE

SET IN MORTAR -

2'-6"

SUITABLE BACKFILL MATERIAL COMPACTED

SAND BEDDING

4" SCH. 40 PVC TELEPHONE CONDUIT

UNDISTURBED EARTH-

5" SCH. 40 PVC

ELECTRIC CONDUIT

SERVICE PROVIDER.

ENCASED 6" OF CONCRETE ON ALL SIDES.

4" SCH. 40 PVC

AS SPECIFIED -

ADJ. TO GRADE W/ BRICK AS REQUIRED

FOR REGISTRY OF DEEDS USE ONLY

Winter GEC, LLC

Design Consultants Inc.

Somerville - Quincy - Newburyport

www.dci-ma.com

34 WINTER STREET NEWBURYPORT, MA 01950 978-270-8626

18 BOYD DRIVE, SUBDIVISION

PREPARED FOR **EVERGREEN**

CON.COM. PLANS REVIEW COMMENTS 11/16/17



TYPICAL SECTIONS, **DETAILS &** NOTES 2

DR BY: WAK CHK BY: SBS PROJ NO: 2015-063

PROJECT TEAM

NEWBURYPORT, MA

COMMONS, LLC

PROJECT INFO

DESCRIPTION DATE

NEWBURYPORT PLANNING BOARD

SCALE: N.T.S.

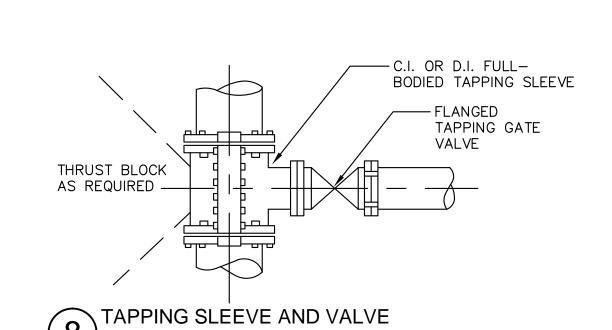
PLANNING BOARD HAS BEEN RECEIVED AND RECORDED AT THIS OFFICE AND NO NOTICE OF APPEAL WAS RECEIVED DURING THE TWENTY DAYS NEXT AFTER SUCH RECEIPT AND RECORDING OF SAID DATE: June 2, 2017 DATE

DATE

3. PLACE BOARD IN FRONT OF ALL PLUGS BEFORE POURING THRUST BLOCKS. 4. WHERE M.J. PIPE IS USED, M.J. PLUG WITH RETAINER GLANDS MUST BE USED, GRIP RING, MEGA-LUG OR STAR GRIP ---NEWBURYPORT STANDARD WATEROUS PACER WB-67 PER NEBURYPORT DPS SPECIIFICATION HYDRANT MARKER SAFETY RED. PENTAGONAL OPERATING NUT SHALL BE 1 5/8" POINT TO FLAT, OPENING RIGHT. 2-2 1/2" HOSE AND 1-4 1/2" PUMP NOZZELS, N.S. THREADS.— GATE BOX BY CENTRAL FOUNDRY 5664-S, FLANGE AT TOP OF TOP SECTION, OR EQUAL— ALL COMPACTION BY MECHANICAL MEANS BLOCK TO UNDISTURBED

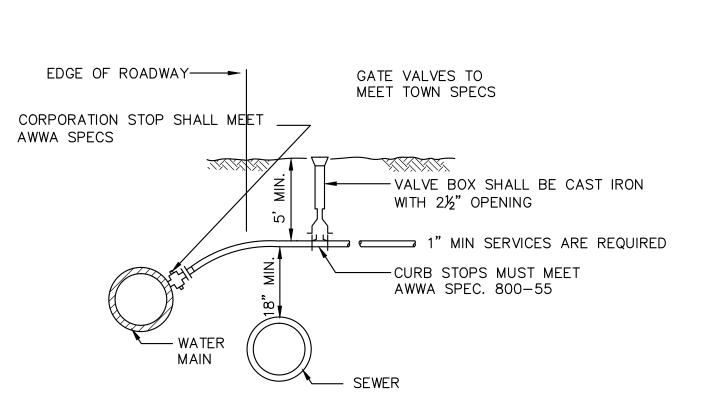
'MET' PATTERN GATE VALVE, M.J., 2" SQUARE OPERATING NUT, OPEN RIGHT, EPOXY COATED -6" C.L.D.I. PIPE (CLASS 52) — HYDRANT SET ON FLAT STONE OR CONCRETE PAD. PLACE 1/2 CUBIC YARD OF PEA STONE AROUND HYDRANT DRAIN AND COVER STONES WITH 8-MIL POLYETHELENE—

HYDRANT GATE VALVE ASSEMBLY



MUNICIPAL STANDARD CATCH BASIN FRAME AND COVER-FINISHED GRADE SET IN MORTAR -- CONCRETE COLLAR ADJ. TO GRADE W/ BRICK AS REQUIRED 2'-6" MASTIC GASKET MANHOLE JOINTS MATERIAL ALL
PRECAST CONCRETE TO CONFORM
TO ASTM C-478 CONC. OF 4000 PSI AT 28 DAYS 6" SCREENED

PRECAST CONCRETE DRAIN MANHOLE



1. COPPER TUBING SHALL MEET AWWA SPEC. 76-CR TYPE K OR FEDERAL SPEC. WWT-799 TYPE K.

2. 1/2" AND LARGER SERVICES ARE REQUIRED TO HAVE A SADDLE.

TYPICAL SERVICE CONNECTION
NOT TO SCALE

24" STEEL FRAME AND GAS TIGHT COVER COVER SHALL BE CAST WITH THE WORD "SEWER" IN THE TOP, LEBARON LA246 -SET FRAME IN FULL MOTAR BED AND CONC. COLLAR -GROOVED TOP RING - STEEL REINFORCING TO MEET A.S.T.M. AND A.A.S.H.T.O. SPECIFICATIONS FOR H-20 LOADING ■──STANDARD BARREL SECTION MONOLITHIC BASE 5"
SECTION 4' DIA.

WITH BRICK AND MOTAR TO PROVIDE 8" CHANNEL

INVERT WITH BRICK AND MOTAR

UNDISTURBED SOIL 1. INVERT AND SHELF TO BE PLACED AFTER LEAKAGE TEST

2. SHELF TO HAVE 2% MINIMUM PITCH TOWARDS CHANNEL

3. BITUMINOUS WATERPROOF COATING TO BE APPLIED TO ENTIRE EXTERIOR OF MANHOLE.

SEWER MANHOLE

CONCRETE COLLAR ---

ECCENTRIC CONE

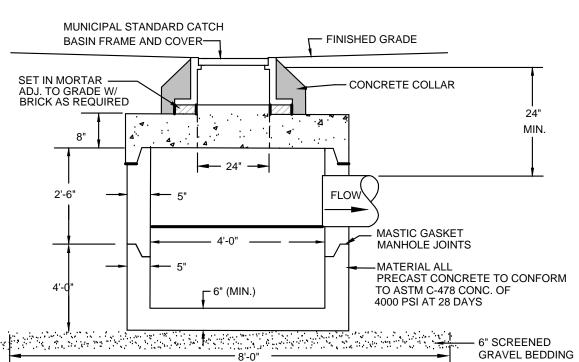
SECTION ——

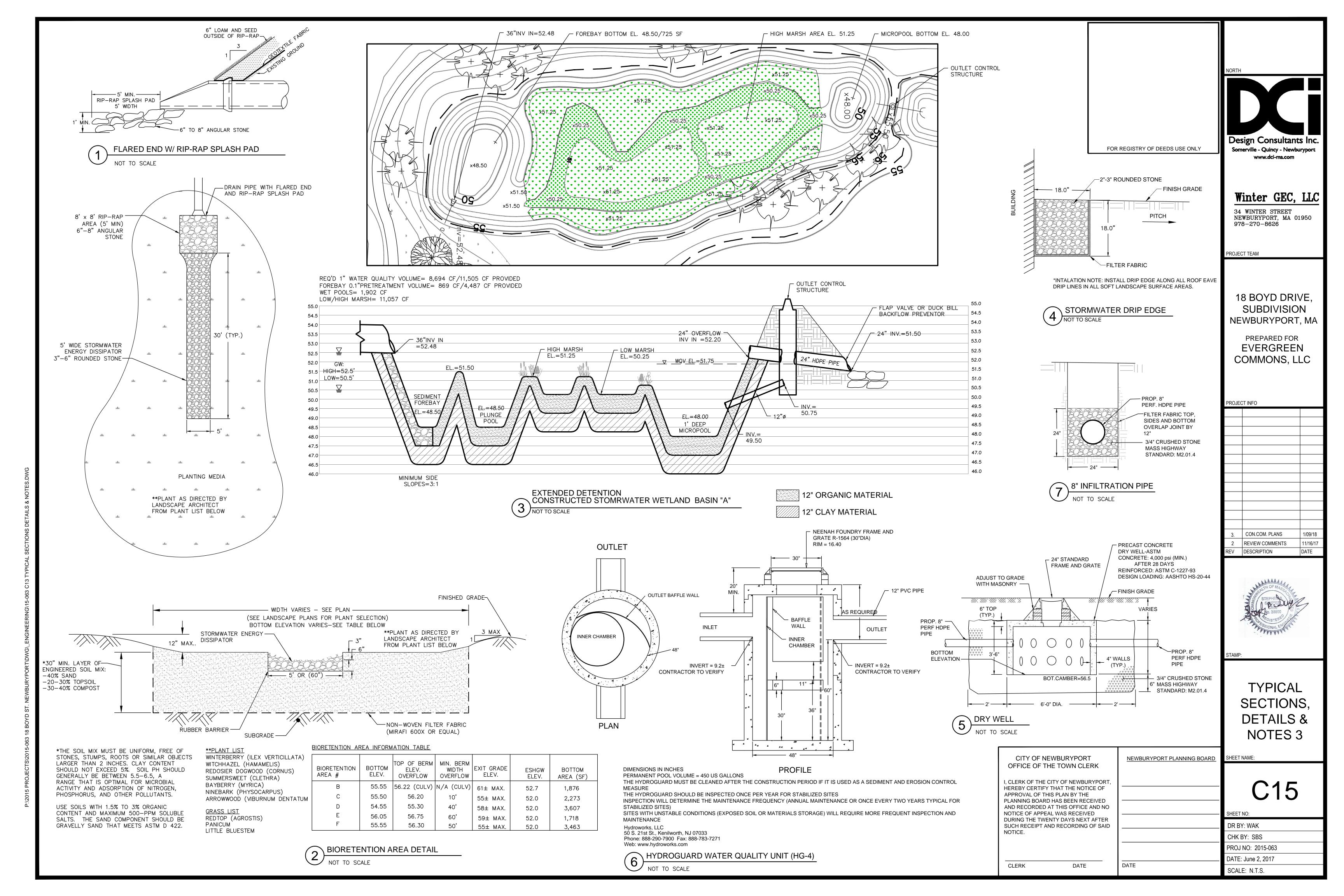
BUTYL RUBBER SEAL

NPC FLEXIBLE SEALS—FOR PIPE TO MANHOLE

CONNECTIONS

CRUSHED GRAVEL-





PROJECT NAME AND LOCATION PORT PLACE, 18 BOYD DRIVE NEWBURYPORT, MASSACHUSETTS

THE PROJECT CONSISTS OF SUBDIVIDING A 36.8 ACRE PARCEL AND THE CONSTRUCTION OF ASSOCIATED SITE IMPROVEMENTS INCLUDING ROADWAY, DRAINAGE SYSTEMS AND UNDERGROUND UTILITIES.

SOIL CHARACTERISTICS

THE EXISTING SITE IS PRESENTLY DEVLELPOED WITH A PORTION OF UNDEVELOPED WOODED ARAE. SOILS CONSIST PRIMARILY OF WELL DRAINING SOILS.

THE TOTAL AREA TO BE DISTURBED IS APPROXIMATELY 20± ACRES.

SEQUENCE OF MAJOR ACTIVITIES

INSTALL TEMPORARY EROSION CONTROL SILT FENCE, STABILIZED CONSTRUCTION ENTRANCE, CLEAR CONSTRUCTION STAGING AREA AND LANDSCAPE PLANTING IN OPEN SPACES OUTSIDE OF DISTURBED AREAS. SEE SITE PLANS FOR LOCATIONS.

- DEMOLISH EXISTING BUILDING, UTILITIES AND PAVEMENT AND FILL SITE TO ROUGH GRADE.
- CLEAR AND GRUB SITE. NO TREES TO BE CLEARED NOT INDICATED ON PLANS. CONTRACTOR SHALL NOTIFY NEWBURYPORT PLANNING DEPARTMENT OF ANY VARIATION FROM PLAN OR ADDITIONAL TREE CUTTING OR CLEARING REQUIREMENTS DUE TO UNFORESEEN SITE CONDITIONS.
- ROUGH GRADING OF SITE. CONSTRUCT ACCESS DRIVES, STORMWATER SYSTEM, UTILITIES AND ROUGH GRADE BUILDING LOTS. WHEN ALL CONSTRUCTION ACTIVITY IS COMPLETE AND SITE IS STABILIZED, REMOVE ALL HAY BALES,

EROSION AND SEDIMENT CONTROLS AND STABILIZATION PRACTICES

STABILIZATION: AN AREA SHALL BE CONSIDERED STABILIZED ONCE ONE OF THE FOLLOWING HAS OCCURRED:

BASE COURSE GRAVELS ARE INSTALLED IN AREAS TO BE PAVED

A MINIMUM OF 85% VEGETATIVE GROWTH HAS BEEN ESTABLISHED

SILT FENCES AND SEDIMENT THAT HAS BEEN TRAPPED BY THESE DEVICES.

A MINIMUM OF 3" OF NON-EROSIVE MATERIAL SUCH AS STONE OR RIP-RAP HAS BEEN INSTALLED EROSION CONTROL BLANKETS HAVE BEEN PROPERLY INSTALLED.

STABILIZATION SHALL BE INITIATED ON ALL LOAM STOCKPILES AND DISTURBED AREAS WHERE CONSTRUCTION ACTIVITY WILL NOT OCCUR FOR MORE THAN THIRTY (30) CALENDAR DAYS BY THE FOURTEENTH (14TH) DAY AFTER CONSTRUCTION ACTIVITY HAS PERMANENTLY OR TEMPORARILY CEASED IN THAT AREA. ALL DISTURBED AREAS SHALL BE STABILIZED WITHIN 60 DAYS OF INITIAL DISTURBANCE. ALL CUT AND FILL SLOPES AND ROADWAYS SHALL BE STABILIZED WITHIN 72 HOURS OF ACHIEVING GRADE. STABILIZATION MEASURES TO BE USED INCLUDE:

> TEMPORARY SEEDING. MULCHING. STONE RIP RAP. JUTE MATTING.

DURING CONSTRUCTION, RUNOFF WILL BE DIVERTED AROUND THE SITE WITH EARTH DIKES, PIPING OR STABILIZED CHANNELS WHERE POSSIBLE. SHEET RUNOFF FROM THE SITE WILL BE FILTERED THROUGH HAY BALE BARRIERS AND/OR SILT FENCES. ALL STORM DRAIN INLETS SHALL BE PROVIDED WITH BARRIER FILTERS. ALL CATCH BASINS WILL BE COVERED WITH A GEOTEXTILE FABRIC PRIOR TO THE BASE PAVEMENT COURSE BEING PLACED. STONE RIP RAP SHALL BE PROVIDED AT THE OUTLETS OF DRAINAGE PIPES WHERE EROSIVE VELOCITIES ARE ENCOUNTERED.

OFF SITE VEHICLE TRACKING

STABILIZED CONSTRUCTION ENTRANCES SHALL BE INSTALLED AT ALL EGRESSES TO THE SITE AND MAINTAINED FOR THE DURATION OF CONSTRUCTION.

AS INDICATED IN THE SEQUENCE OF MAJOR ACTIVITIES THE AND SILT FENCES SHALL BE INSTALLED PRIOR TO COMMENCING ANY CLEARING OR GRADING OF THE SITE. STRUCTURAL CONTROLS SHALL BE INSTALLED CONCURRENTLY WITH THE APPLICABLE ACTIVITY. AREAS WHERE CONSTRUCTION ACTIVITY TEMPORARILY CEASES FOR MORE THAN THIRTY (30) DAYS WILL BE STABILIZED WITH A TEMPORARY SEED AND MULCH WITHIN FOURTEEN (14) DAYS OF THE LAST DISTURBANCE. ONCE CONSTRUCTION ACTIVITY CEASES PERMANENTLY IN AN AREA, SILT FENCES AND HAY BALE BARRIERS AND ANY EARTH/DIKES WILL BE REMOVED ONCE PERMANENT MEASURES ARE ESTABLISHED.

INSTALLATION, MAINTENANCE AND INSPECTION PROCEDURES OF EROSION AND SEDIMENT CONTROLS

GENERAL

- 1. ALL CONTROL MEASURES WILL BE INSPECTED AT LEAST ONCE EACH WEEK AND FOLLOWING ANY STORM EVENT OF 0.5 INCHES OR GREATER.
- 2. A MAINTENANCE INSPECTION REPORT WILL BE MADE AFTER EACH INSPECTION.
- 3. THE CONTRACTOR'S SITE SUPERINTENDENT WILL BE RESPONSIBLE FOR INSPECTIONS, MAINTENANCE AND REPAIR ACTIVITIES, AND FILLING OUT THE INSPECTION AND MAINTENANCE REPORT.

- 1. STABILIZATION OF ALL SWALES, DITCHES AND PONDS IS REQUIRED PRIOR TO DIRECTING
- 2. ALL MEASURES WILL BE MAINTAINED IN GOOD WORKING ORDER 10 BX REPAIR IS NECESSARY, IT WILL BE INITIATED WITHIN 24 HOURS OF REPORT.
- 3. BUILT UP SEDIMENT WILL BE REMOVED FROM SILT FENCE OR HAY BALE BARRIERS WHEN IT HAS REACHED ONE THIRD THE HEIGHT OF THE FENCE OR BALE.
- 4. ALL DIVERSION DIKES WILL BE INSPECTED AND ANY BREACHES PROMPTLY REPAIRED. 5. TEMPORARY SEEDING AND PLANTING WILL BE INSPECTED FOR BARE SPOTS, WASHOUTS, AND

DISTURBED CONTRIBUTING AREA SHOULD NOT EXCEED 0.25 ACRES PER 100 LINEAR FEET OF FILTER BARRIER.

1. STRAW/HAY BALES A. SHEET FLOW APPLICATIONS

UNHEALTHY GROWTH.

- BALES SHALL BE PLACED IN A SINGLE ROW, LENGTHWISE ON THE CONTOUR, WITH ENDS OF ADJACENT BALES TIGHTLY ABUTTING ONE ANOTHER.
- ALL BALES SHALL BE EITHER WIRE-BOUND OR STRING-TIED. BALES SHALL BE INSTALLED SO THAT BINDINGS ARE ORIENTED AROUND THE SIDES RATHER THAN ALONG THE TOPS AND BOTTOMS OF THE BALES TO PREVENT DETERIORATION OF THE BINDINGS.
- THE BARRIER SHALL BE ENTRENCHED AND BACKFILLED. A TRENCH SHALL BE EXCAVATED THE WIDTH OF A BALE AND THE LENGTH OF THE PROPOSED BARRIER TO A MINIMUM DEPTH OF FOUR (4) INCHES. AFTER THE BALES ARE STAKED AND CHINKED. THE EXCAVATED SOIL SHALL BE BACKFILLED AGAINST THE BARRIER. BACKFILL SOIL SHALL CONFORM TO THE GROUND LEVEL ON THE DOWNHILL SIDE AND SHALL BE BUILT UP TO FOUR (4) INCHES AGAINST THE UPHILL SIDE OF THE BARRIER. IDEALLY, BALES SHOULD BE PLACED TEN (10) FEET AWAY FROM THE TOE OF SLOPE.
- EACH BALE SHALL BE SECURELY ANCHORED BY AT LEAST TWO (2) STAKES OR REBARS DRIVEN THROUGH THE BALE. THE FIRST SAKE IN EACH BALE SHALL BE DRIVEN TOWARD THE PREVIOUSLY LAID BALE TO FORCE THE BALES TOGETHER. STAKES OR REBARS SHALL BE DRIVEN DEEP ENOUGH INTO THE GROUND TO SECURELY ANCHOR THE BALES.
- THE GAPS BETWEEN BALES SHALL BE CHINKED (FILLED BY WEDGING) WITH STRAW/HAY TO PREVENT WATER FROM ESCAPING BETWEEN THE BALES.

SYNTHETIC FILTER FABRIC SHALL BE A PERVIOUS SHEET OF PROPYLENE, NYLON, POLYESTER OR ETHYLENE YARN AND SHALL BE CERTIFIED BY THE MANUFACTURER OR

SUPPLIER AS CONFORMING TO THE FOLLOWING REQUIREMENTS: PHYSICAL PROPERTY TEST REQUIREMENTS FILTERING EFFICIENCY VTM-51 75% MINIMUM TENSILE STRENGTH AT VTM-52 EXTRA STRENGTH 20% MAXIMUM ELONGATION* 50 LB/LIN IN (MIN) STANDARD STRENGTH 30 LB/LIN IN (MIN)

FLOW RATE 0.3 GAL/SF/MIN (MIN) REQUIREMENTS REDUCED BY 50 PERCENT AFTER SIX (6) MONTHS OF

SYNTHETIC FILTER FABRIC SHALL CONTAIN ULTRAVIOLET RAY INHIBITORS AND STABILIZERS TO PROVIDE A MINIMUM OF SIX (6) MONTHS OF EXPECTED USABLE CONSTRUCTION LIFE AT A TEMPERATURE RANGE OF 0 DEGREES F TO 120 DEGREES F. THE HEIGHT OF A SILT FENCE SHALL NOT EXCEED THIRTY-SIX (36) INCHES. THE FILTER FABRIC SHALL BE PURCHASED IN A CONTINUOUS ROLL CUT TO THE LENGTH OF THE BARRIER TO AVOID THE USE OF JOINTS. WHEN JOINTS ARE NECESSARY, FILTER CLOTH SHALL BE SPLICED TOGETHER ONLY AT SUPPORT POST, WITH A

POSTS SHALL BE SPACED A MAXIMUM OF TEN (10) FEET APART AT THE BARRIER LOCATION AND DRIVEN SECURELY INTO THE GROUND (MINIMUM OF 16 INCHES).

MINIMUM SIX (6) INCH OVERLAP, AND SECURELY SEALED.

A TRENCH SHALL BE EXCAVATED APPROXIMATELY SIX (6) INCHES WIDE AND SIX (6) INCHES DEEP ALONG THE LINE OF POSTS AND UPSLOPE FROM THE BARRIER.

- WHEN STANDARD STRENGTH FILTER FABRIC IS USED, A WIRE MESH SUPPORT FENCE SHALL BE FASTENED SECURELY TO THE UPSLOPE SIDE OF THE POSTS USING HEAVY DUTY WRE STAPLES AT LEAST ONE (1) INCH LONG, TIE WIRES OR HOG RINGS. THE WIRE SHALL EXTEND NO MORE THAN 36 INCHES ABOVE THE ORIGINAL GROUND SURFACES. THE "STANDARD STRENGTH" FILTER FABRIC SHALL BE STAPLED OR WIRED TO THE FENCE, AND EIGHT (8) INCHES OF THE FABRIC SHALL BE EXTENDED INTO THE
- TRENCH. THE FABRIC SHALL NOT EXTEND MORE THAN 36 INCHES ABOVE THE ORIGINAL GROUND SURFACE. FILTER FABRIC SHALL NOT BE STAPLED TO EXISTING H. WHEN EXTRA STRENGTH FILTER FABRIC AND CLOSER POST SPACING ARE USED, THE WIRE MESH SUPPORT FENCE MAY BE ELIMINATED. IN SUCH A CASE, THE FILTER
- FABRIC IS STAPLED OR WIRED DIRECTLY TO THE POSTS WITH ALL OTHER PROVISIONS OF ITEM (G) APPLYING. THE TRENCH SHALL BE BACKFILLED AND THE SOIL COMPACTED OVER THE FILTER
- SILT FENCES SHALL BE REMOVED WHEN THEY HAVE SERVED THEIR USEFUL PURPOSE, BUT NOT BEFORE THE UPSLOPE AREAS HAS BEEN PERMANENTLY

STABILIZED.

SEQUENCE OF INSTALLATION SEDIMENT BARRIERS SHALL BE INSTALLED PRIOR TO ANY SOIL DISTURBANCE OF THE CONTRIBUTING DRAINAGE AREA ABOVE THEM.

- A. STRAW/HAY BALE BARRIER AND SILT FENCE BARRIERS SHALL BE INSPECTED IMMEDIATELY AFTER EACH RAINFALL AND AT LEAST DAILY DURING PROLONGED RAINFALL. THEY SHALL BE REPAIRED IF THERE ARE ANY SIGNS OF EROSION OR SEDIMENTATION BELOW THEM. ANY REQUIRED REPAIRS SHALL BE MADE IMMEDIATELY. IF THERE ARE SIGNS OF UNDERCUTTING AT THE CENTER OR THE EDGES, OR IMPOUNDING OF LARGE VOLUMES OF WATER BEHIND THEM, SEDIMENT BARRIERS SHALL BE REPLACED WITH A TEMPORARY CHECK DAM.
- SHOULD THE FABRIC ON A SILT FENCE OR FILTER BARRIER DECOMPOSE OR BECOME INEFFECTIVE PRIOR TO THE END OF THE EXPECTED USABLE LIFE AND THE BARRIER STILL IS NECESSARY, THE FABRIC SHALL BE REPLACED PROMPTLY.
- SEDIMENT DEPOSITS SHOULD BE REMOVED AFTER EACH STORM EVENT. THEY MUST BE REMOVED WHEN DEPOSITS REACH APPROXIMATELY ONE THIRD (1/3) THE
- ANY SEDIMENT DEPOSITS REMAINING IN PLACE AFTER THE SILT FENCE OR FILTER BARRIER IS NO LONGER REQUIRED SHALL BE DRESSED TO CONFORM WITH THE EXISTING GRADE, PREPARED AND SEEDED.

MULCHING TIMING

IN ORDER FOR MULCH TO BE EFFECTIVE, IT MUST BE IN PLACE PRIOR TO MAJOR STORM EVENTS. THERE ARE TWO (2) TYPES OF STANDARDS WHICH SHALL BE USED TO ASSURE THIS. A. APPLY MULCH PRIOR TO ANY STORM EVENT.

THIS IS APPLICABLE WHEN WORKING WITHIN 100 FEET OF WETLANDS. IT WILL BE NECESSARY TO CLOSELY MONITOR WEATHER PREDICTIONS, USUALLY BY CONTACTING THE NATIONAL WEATHER SERVICE IN CONCORD, TO HAVE ADEQUATE WARNING OF SIGNIFICANT STORMS.

B. REQUIRED MULCHING WITHIN A SPECIFIED TIME PERIOD.

THE TIME PERIOD CAN RANGE FROM 14 TO 21 DAYS OF INACTIVITY ON A AREA, THE LENGTH OF TIME VARYING WITH SITE CONDITIONS. PROFESSIONAL JUDGMENT SHALL BE USED TO EVALUATE THE INTERACTION OF SITE CONDITIONS (SOIL ERODIBILITY, SEASON OF YEAR, EXTENT OF DISTURBANCE, PROXIMITY TO SENSITIVE RESOURCES, ETC.) AND THE POTENTIAL IMPACT OF EROSION ON ADJACENT AREAS TO CHOOSE AN APPROPRIATE TIME RESTRICTION.

GUIDELINES FOR WINTER MULCH APPLICATION.

WHEN MULCH IS APPLIED TO PROVIDE PROTECTION OVER WINTER (PAST THE GROWING SEASON) IT SHALL BE AT A RATE OF 6,000 POUNDS OF HAY OR STRAW PER ACRE. A TACKIFIER MAY BE ADDED TO THE MULCH.

ALL MULCHES MUST BE INSPECTED PERIODICALLY, IN PARTICULAR AFTER RAINSTORMS, TO CHECK FOR RILL EROSION. IF LESS THAN 90% OF THE SOIL SURFACE IS COVERED BY MULCH, ADDITIONAL MULCH SHALL BE IMMEDIATELY APPLIED.

FOR PERMANENT MEASURES AND PLANTINGS FROM EARLY SPRING TO SEPTEMBER 30:

AFTER ROUGH GRADING OF THE SUBGRADE HAS BEEN COMPLETED AND APPROVED, THE SUB GRADE SURFACE SHALL BE SCARIFIED TO A DEPTH OF FOUR INCHES. THEN FURNISH AND INSTALL A LAYER OF LOAM PROVIDING A ROLLED FOUR INCH THICKNESS. ANY DEPRESSIONS WHICH MAY OCCUR DURING ROLLING SHALL BE FILLED WITH ADDITIONAL LOAM, REGRADED AND REROLLED COMPLETE THE WORK UNDER THIS SECTION SHALL BE SUPPLIED BY THE SITE SUBCONTRACTOR.

ALL LARGE STIFF CLODS, LUMPS, BRUSH, ROOTS, DEBRIS, GLASS, STUMPS, LITTER AND OTHER FOREIGN MATERIAL AS WELL AS STONES OVER ONE INCH IN DIAMETER SHALL BE REMOVED FROM THE LOAM AND DISPOSED OF OFF SITE, AND THE LOAM SHALL BE RAKED SMOOTH

THE LOAM SHALL BE PREPARED TO RECEIVE SEED BY REMOVING STONES, FOREIGN OBJECTS AND GRADING TO ELIMINATE WATER POCKETS AND IRREGULARITIES PRIOR TO PLACING SEED. FINISH GRADING SHALL RESULT IN STRAIGHT UNIFORM GRADES AND SMOOTH, EVEN SURFACES WITHOUT IRREGULARITIES TO LOW POINTS.

SHAPE THE AREAS TO THE LINES AND GRADES REQUIRED. THE SITE SUBCONTRACTOR'S ATTENTION IS DIRECTED TO THE SCHEDULING OF LOAMING AND SEEDING OF GRADED AREAS TO PERMIT SUFFICIENT TIME FOR THE STABILIZATION OF THESE AREAS. IT SHALL BE THE SITE SUBCONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE AREAS DURING THE CONSTRUCTION PERIOD AND REGRADE, LOAM AND RESEED ANY DAMAGED AREAS.

ALL AREAS DISTURBED BY CONSTRUCTION WITHIN THE PROPERTY LINES AND NOT COVERED BY STRUCTURES, PAVEMENT, OR MULCH SHALL BE LOAMED AND SEEDED. LIMESTONE SHALL BE THOROUGHLY INCORPORATED INTO THE LOAM LAYER AT A RATE OF 2 TONS PER ACRE IN ORDER TO PROVIDE A PH VALUE OF 5.5 TO 6.5.

IF REQUIRED, FERTILIZER SHALL BE SPREAD ON THE TOP LAYER OF LOAM AND WORKED INTO THE SURFACE. FERTILIZER APPLICATION RATE SHALL BE 500 POUNDS PER ACRE OF 10-20-20 FERTILIZER. USE OF FERTILIZER SHOULD BE AVOIDED IN INFILTRATION AREAS.

SOIL CONDITIONERS AND FERTILIZER SHALL BE APPLIED AT THE RECOMMENDED RATES AND SHALL BE THOROUGHLY WORKED INTO THE LOAM. LOAM SHALL BE RAKED UNTIL THE SURFACE IS FINELY PULVERIZED, SMOOTH AND EVEN, AND THEN COMPACTED TO AN EVEN SURFACE CONFORMING TO THE REQUIRED LINES AND GRADES WITH APPROVED ROLLERS WEIGHING BETWEEN 4 1/2 POUNDS AND 5 1/2 POUNDS PER INCH OF WIDTH

SEED SHALL BE SOWN AT THE RATE SHOWN BELOW. SOWING SHALL BE DONE ON A CALM, DRY DAY, PREFERABLY BY MACHINE, BUT IF BY HAND, ONLY BY EXPERIENCED WORKMEN. IMMEDIATELY BEFORE SEEDING, THE SOIL SHALL BE LIGHTLY RAKED. ONE HALF THE SEED SHALL BE SOWN IN ONE DIRECTION AND THE OTHER HALF AT RIGHT ANGLES TO THE ORIGINAL DIRECTION. IT SHALL BE LIGHTLY RAKED INTO THE SOIL TO A DEPTH NOT OVER 1/4 INCH INCH AND ROLLED WITH A HAND ROLLER WEIGHING NOT OVER 100 POUNDS PER LINEAR FOOT OF WIDTH.

HAY MULCH SHALL BE APPLIED IMMEDIATELY AFTER SEEDING AT A RATE OF 1.5 TO 2 TONS PER ACRE. MULCH THAT BLOWS OR WASHES AWAY SHALL BE REPLACED IMMEDIATELY AND ANCHORED USING APPROPRIATE TECHNIQUES FROM THE EROSION AND SEDIMENT CONTROL HANDBOOK. THE SURFACE SHALL BE WATERED AND KEPT MOIST WITH A FINE SPRAY AS REQUIRED, WITHOUT WASHING AWAY THE SOIL, UNTIL THE GRASS IS WELL ESTABLISHED. ANY AREAS WHICH ARE NOT SATISFACTORILY COVERED WITH GRASS SHALL BE RESEEDED, AND ALL NOXIOUS WEEDS REMOVED.

THE SITE SUBCONTRACTOR SHALL PROTECT AND MAINTAIN THE SEEDED AREAS UNTIL ACCEPTED. INCLUDING CUTTING, AS SPECIFIED HEREIN AFTER UNDER MAINTENANCE AND PROTECTION. UNLESS OTHERWISE APPROVED. SEEDING SHALL BE DONE DURING THE APPROXIMATE PERIODS OF EARLY SPRING TO SEPTEMBER 30, WHEN SOIL CONDITIONS AND WEATHER ARE SUITABLE FOR SUCH WORK.

A GRASS SEED MIXTURE CONTAINING THE FOLLOWING SEED REQUIREMENTS SHALL BE:

CREEPING RED FESCUE KENTUCKY BLUE GRASS 50 85% 97% 97% FEM 100 85% 97% FEM 100 97% FEM 100 SLOPE SEED (USED ON ALL SLOPES GREATER THAN OR EQUAL TO 3:1) CREEPING RED FESCUE 20 85% 96% 76% 76% 76% 76% 76% 76% 76% 76% 76% 7	CENERAL COVER	POUNDS PER ACRE	MINIMUM	MINIMU
100 GERMINATION PURIT SLOPE SEED (USED ON ALL SLOPES GREATER THAN OR EQUAL TO 3:1) CREEPING RED FESCUE 20 85% 96% TALL FESCUE 20 85% 96% RED TOP 2 80% 95%				
CREEPING RED FESCUE 20 85% 96% TALL FESCUE 20 85% 96% RED TOP 2 80% 95%			GERMINATION	PURITY
40	CREEPING RED FESCUE TALL FESCUE	20 20 2	85% 85%	96% 96%

IN NO CASE SHALL THE WEED CONTENT EXCEED 1 PERCENT BY WEIGHT. ALL SEED SHALL COMPLY WITH STATE AND FEDERAL SEED LAWS.

FOR TEMPORARY PLANTINGS AFTER SEPTEMBER TO EARLY SPRING AND FOR TEMPORARY PROTECTION OF DISTURBED AREAS:

- O FOLLOW ABOVE SLOPE, LOAM DEPTH AND GRADING REQUIREMENTS. FERTILIZER SHALL BE SPREAD AND WORKED INTO THE SURFACE AT A
- RATE OF 300 POUNDS PER ACRE. MULCHING AND SEEDING SHALL BE APPLIED AT THE FOLLOWING RATES: WINTER RYE (FALL SEEDING) 2.5 LBS/1,000 S.F. 2 LBS./1,000S.F. OATS (SPRING SEEDING) 1.5 TONS/ACRE

STABILIZED CONSTRUCTION ENTRANCE

SPECIFICATIONS

- AGGREGATE SIZE: USE TWO (2) INCHES STONE, OR RECLAIMED OR RECYCLED
- CONCRETE EQUIVALENT.
- AGGREGATE THICKNESS: NOT LESS THAN SIX (6) INCHES. WIDTH: TEN (10) FOOT MINIMUM, BUT NOT LESS THAN THE FULL WIDTH OF
- POINTS WHERE INGRESS OR EGRESS OCCURS. LENGTH: AS REQUIRED, BUT NOT LESS THAN FIFTY (50) FEET.
- GEOTEXTILE: TO BE PLACED OVER THE ENTIRE AREA TO BE COVERED WITH AGGREGATE.
- PIPING OF SURFACE WATER UNDER ENTRANCE SHALL BE PROVIDED AS REQUIRED. F. CRITERIA FOR GEOTEXTILE: THE FABRICS SHALL BE TREVIA SPUNBOND 1135, MIRAFI 600X OR EQUAL

THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OF SEDIMENT ONTO PUBLIC RIGHT-OF-WAY. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH AGGREGATE WHICH DRAINS INTO AN APPROVED SEDIMENT TRAPPING DEVICE. ALL SEDIMENT SHALL BE PREVENTED FROM ENTERING STORM DRAINS, DITCHES, OR WATERWAYS.

WASTE DISPOSAL

A. WASTE MATERIALS

ALL WASTE MATERIALS WILL BE COLLECTED AND STORED IN SECURELY LIDDED RECEPTACLES. ALL TRASH AND CONSTRUCTION DEBRIS FROM THE SITE WILL BE DEPOSITED IN A DUMPSTER. NO CONSTRUCTION WASTE MATERIALS WILL BE BURIED ON SITE. ALL PERSONNEL WILL BE INSTRUCTED REGARDING THE CORRECT PROCEDURE FOR WASTE DISPOSAL BY THE SUPERINTENDENT.

HAZARDOUS WASTE

ALL HAZARDOUS WASTE MATERIALS WILL BE DISPOSED OF IN THE MANNER SPECIFIED BY LOCAL OR STATE REGULATION OR BY THE MANUFACTURER. SITE PERSONNEL WILL BE INSTRUCTED IN THESE PRACTICES BY THE SUPERINTENDENT.

SANITARY WASTE

ALL SANITARY WASTE WILL BE COLLECTED FROM THE PORTABLE UNITS A MINIMUM OF ONCE PER WEEK BY A LICENSED SANITARY WASTE MANAGEMENT CONTRACTOR.

ADDITIONAL NOTES FOR WINTER CONSTRUCTION.

- A) ALL PROPOSED POST-DEVELOPMENT LANDSCAPED AREAS WHICH DO NOT EXHIBIT A MINIMUM OF 85% VEGETATIVE GROWTH BY NOVEMBER 15TH, OR WHICH ARE DISTURBED AFTER NOVEMBER 15TH, SHALL BE STABILIZED BY SEEDING AND INSTALLING EROSION CONTROL BLANKETS ON SLOPES GREATER THAN 4:1 AND SEEDING AND PLACING 3 TO 4 TONS OF MULCH PER ACRE. SECURED WITH ANCHORED NETTING, ELSEWHERE. THE PLACEMENT OF EROSION CONTROL BLANKETS OR MULCH AND NETTING SHALL NOT OCCUR OVER ACCUMULATED SNOW OR FROZEN GROUND.
- B) ALL DITCHES OR SWALES WHICH DO NOT EXHIBIT A MINIMUM OF 85% VEGETATIVE GROWTH BY NOVEMBER 15TH, OR WHICH ARE DISTURBED AFTER NOVEMBER 15TH, SHALL BE STABILIZED WITH STONE OR EROSION CONTROL BLANKETS APPROPRIATE FOR THE
- C) AFTER NOVEMBER 15TH, ALL TRAVEL SURFACES SHALL BE PROTECTED WITH A MINIMUM OF 3-INCHES OF CRUSHED GRAVEL OR IF CONSTRUCTION IS TO CONTINUE THROUGH THE WINTER SEASON BE CLEARED OF ANY ACCUMULATED SNOWFALL AFTER EACH STORM EVENT

<u>DUST_CONTROL</u>

THE CONTRACTOR SHALL BE RESPONSIBLE TO CONTROL DUST THROUGHOUT THE CONSTRUCTION PERIOD DUST CONTROL METHODS SHALL INCLUDE, BUT NOT LIMITED TO SPRINKLING WATER ON EXPOSED AREAS, COVERING LOADED DUMP TRUCKS LEAVING THE SITE, AND TEMPORARY MULCHING. DUST CONTROL MEASURES SHALL BE UTILIZED SO AS TO PREVENT THE MIGRATION OF DUST FROM THE SITE TO ABUTTING AREAS.

SEDIMENT SHOULD BE REMOVED AT LEAST ANNUALLY TO PREVENT CLOGGING.

SILT SACK

NOT TO SCALE

BIORETENTION BASIN MAINTENANCE

1" REBAR FOR BAG

(REBAR NOT INCLUDED)

REMOVAL FROM INLET

OPTIONAL OVERFLOW

SILTSACK

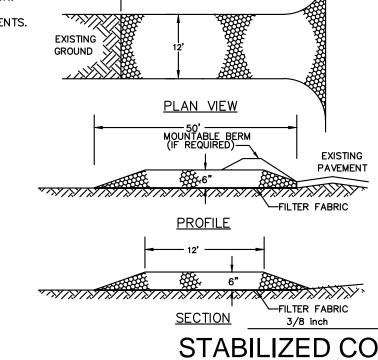
DUMP LOOPS

(REBAR NOT INCLUDED)

INSTALLATION DETAIL

A) INFILTRATION BASINS SHALL NOT BE USED AS TEMPORARY SEDIMENTATION BASINS . ALL DRAINAGE SHOULD BE DIRECTED AWAY FROM AREAS DESIGNATED FOR BIORETENTION WITH TEMPORARY SEDIMENT CONTROL BASINS, DIVERSION

- B) FERTILIZERS SHOULD NOT BE OVER INFILTRATION BED UNLESS ABSOLUTELY NECESSARY TO ESTABLISH VEGETATION.
- C) INFILTRATION BASINS SHOULD BE INSPECTED A FEW TIMES A YEAR AND PARTICULARLY AFTER LARGE STORM EVENTS.



CONSTRUCTION SPECIFICATIONS STONE SIZE - NHDOT STANDARD STONE SIZE #4 - SECTION 703 OF NHDOT STANDARD SPECIFICATIONS. (SEE GRADATION TABLE) LENGTH - 50 FOOT MINIMUM.

<u>FILTER FABRIC</u> — MIRAFI 600X OR APPROVED EQUAL. HE GRAVEL SHALL BE PLACED TO THE SPECIFIED DIMENSIONS.

 $\frac{\text{MAINTENANCE}}{\text{MAINTENANCE}} - \text{THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY.}$ THIS WILL REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE OR ADDITIONAL LENGTH AS CONDITIONS DEMAND AND REPAIR AND/OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT. ALL SEDIMENT SPILLED, DROPPED, WASHED OR TRACKED ONTO PUBLIC RIGHTS-OF-WAY MUST BE REMOVED IMMEDIATELY.

SIEVE SIZE BY WEIGHT 1 1/2 inches 3/4 inch

STABILIZED CONSTRUCTION ENTRANCE NOT TO SCALE

(BY ACF ENVIRONMENTAL) OR "STREAM GUARD" (BY FOSS ENVIRONMENTAL SERVICES) OR EQUAL. 2. INSERT TO BE EMPTIED AND PROPERLY DISPOSED OF WHEN IT IS 1/2 FULL OF SEDIMENT. INSPECT INSERT AFTER ALL RAINFALL EVENTS, REPAIR AND MAINTAIN AS REQUIRED.

1. CATCH BASIN PROTECTION TO BE "SILTSACK"

REGULAR FLOW SILTSACK @ (FOR AREAS OF LOW TO MODERATE PRECIPITATION AND RUN-OFF)

OPERTIES .	TEST METHOD	UNITS	
RAB TENSILE STRENGTH RAB TENSILE ELONGATION UNCTURE ULLEN BURST RAPEZOID TEAR V RESISTANCE PPARENT OPENING SIZE LOW RATE ERMITTIVITY	ASTM D-	-4632 20 -4833 12 -3786 80 -4533 12 -4355 80 -4751 40 -4491 40	0 LBS 0 % 0 LBS 0 PSI 0 LBS 0 LBS 0 W US SIEVE GAL/MIN/SQ FT 55 SEC -1

I, CLERK OF THE CITY OF NEWBURYPORT, HEREBY CERTIFY THAT THE NOTICE OF APPROVAL OF THIS PLAN BY THE PLANNING BOARD HAS BEEN RECEIVED AND RECORDED AT THIS OFFICE AND NO NOTICE OF APPEAL WAS RECEIVED DURING THE TWENTY DAYS NEXT AFTER SUCH RECEIPT AND RECORDING OF SAID NOTICE.

CLERK

DR BY: WAK CHK BY: SBS PROJ NO: 2015-063 DATE: June 2, 2017 SCALE: N.T.S.

CITY OF NEWBURYPORT OFFICE OF THE TOWN CLERK

FILTER BARRIERS SHALL BE REMOVED WHEN THEY HAVE SERVED THEIR USEFUL PURPOSE, BUT NOT BEFORE THE 9. FILTER BARRIERS SHALL BE INSPECTED IMMEDIATELY AFTER EACH RAINFALL, AND AT LEAST DAILY DURING PROLONGED 10. SHOULD THE FABRIC DECOMPOSE OR BECOME INEFFECTIVE PRIOR TO THE END OF THE EXPECTED USABLE LIFE AND THE BARRIER STILL BE NECESSARY, THE FABRIC SHALL BE REPLACED PROMPTLY.

PROJECT INFO

FOR REGISTRY OF DEEDS USE ONLY

STURBED AREA TO B

<u>THICKNESS</u> - SIX (6) INCHES (MINIMUM). <u>WDTH</u> - 12' MINIMUM

THE GEOTEXTILE FABRIC SHALL MEET THE DESIGN CRITERIA FOR BEST MANAGEMENT PRACTICE FOR SILT FENCES.

WHEN JOINTS ARE NECESSARY, FILTER CLOTH SHALL BE SPLICED TOGETHER ONLY AT A SUPPORT POST, WITH A MINIMUM 6-INCH OVERLAP, AND SECURELY SEALED. SEE MANUFACTURER'S RECOMMENDATIONS.

POSTS SHALL BE SPACED A MAXIMUM OF 10 FEET APART AT THE BARRIER LOCATION AND DRIVEN SECURELY INTO THE

GROUND (MINIMUM OF 16 INCHES). WHEN EXTRA STRENGTH FABRIC IS USED WITHOUT THE WIRE SUPPORT FENCE, POST SPACING SHALL BE AS MANUFACTURER RECOMMENDS.

A TRENCH SHALL BE EXCAVATED APPROXIMATELY 6 INCHES WIDE AND 6 INCHES DEEP ALONG THE LINE OF POSTS AND

THE FABRIC SHALL NOT EXTEND MORE THAN 36 INCHES ABOVE THE ORIGINAL GROUND SURFACE, AND WILL EXTEND TO

11. SEDIMENT DEPOSITS SHOULD BE REMOVED WHEN THEY REACH APPROXIMATELY ONE-THIRD THE HEIGHT OF THE BARRIER

12. ANY SEDIMENT DEPOSITS REMAINING IN PLACE AFTER THE SILT FENCE OR FILTER BARRIER IS NO LONGER REQUIRED,

A MINIMUM OF 8 INCHES INTO THE TRENCH. FILTER FABRIC SHALL NOT BE STAPLED INTO EXISTING TREES.

THE HEIGHT OF THE BARRIER SHALL NOT EXCEED 36 INCHES.

UPSLOPE AREA HAS BEEN PERMANENTLY STABILIZED.

UPSLOPE FROM THE BARRIER IN ACCORDANCE WITH RECOMMENDATIONS.

RAINFALL, ANY REQUIRED REPAIRS SHALL BE MADE IMMEDIATELY.

THE TRENCH SHALL BE BACKFILLED AND THE SOIL COMPACTED OVER THE FILTER FABRIC.

SHALL BE DRESSED TO CONFORM TO THE EXISTING GRADE, PREPARED AND SEEDED.

INSTALLATION - THE AREA OF THE ENTRANCE SHOULD BE CLEARED OF ALL VEGETATION. ROOTS, AND OTHER OBJECTIONABLE MATERIAL. A ROAD STABLILIZATION FILTER CLOTH CAN BE PLACED ON THE SUBGRADE PRIOR TO THE GRAVEL PLACEMENT TO PREVENT PUMPING.

CRUSHED STONE GRADATION TABLE

NEWBURYPORT PLANNING BOARD SHEET NAME:

DATE

18 BOYD DRIVE. SUBDIVISION NEWBURYPORT, MA

PREPARED FOR **EVERGREEN** COMMONS, LLC

Design Consultants Inc.

Somerville - Quincy - Newburyport

www.dci-ma.com

Winter GEC, LLO

NEWBURYPORT, MA 01950

978-270-8626

ROJECT TEAM

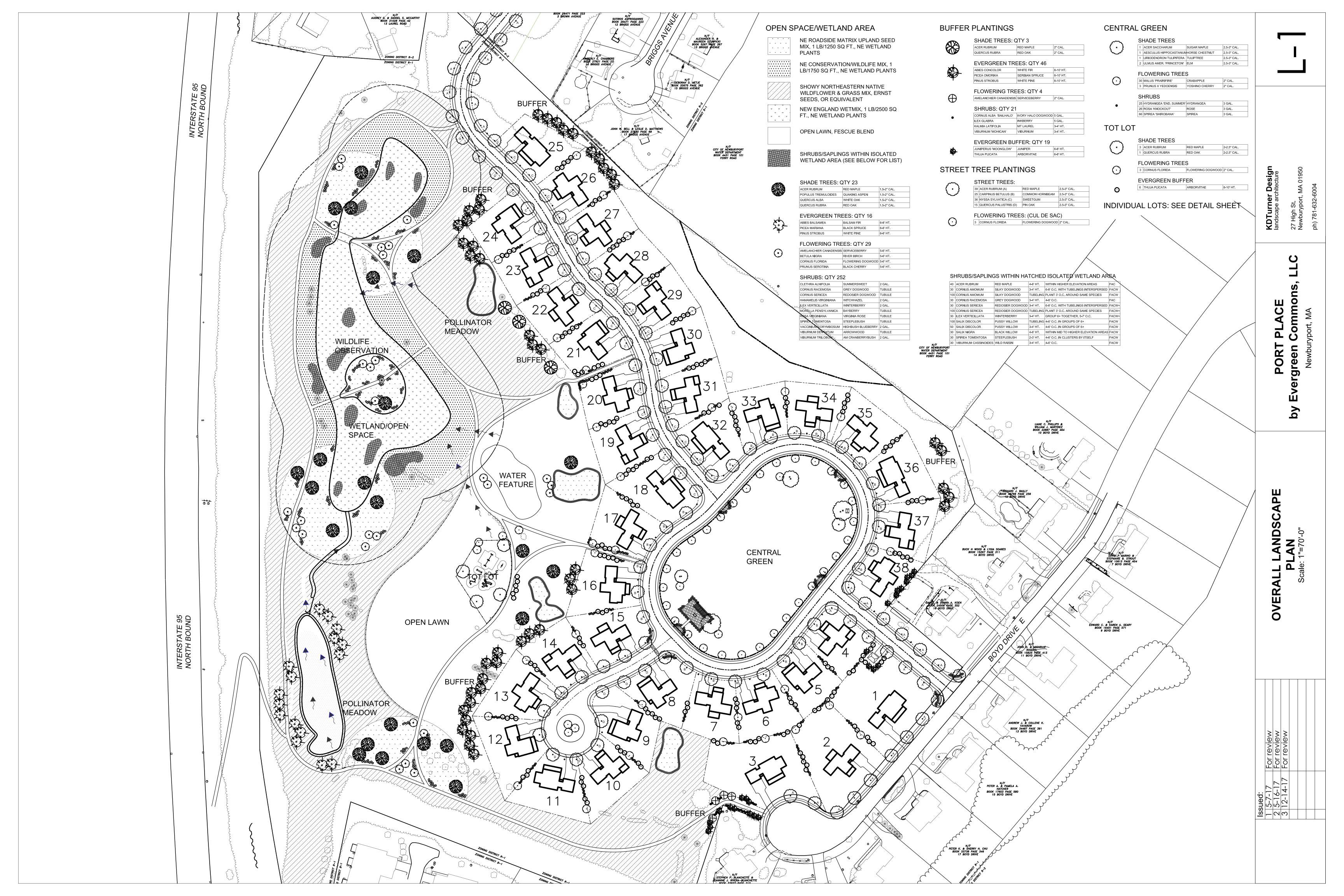
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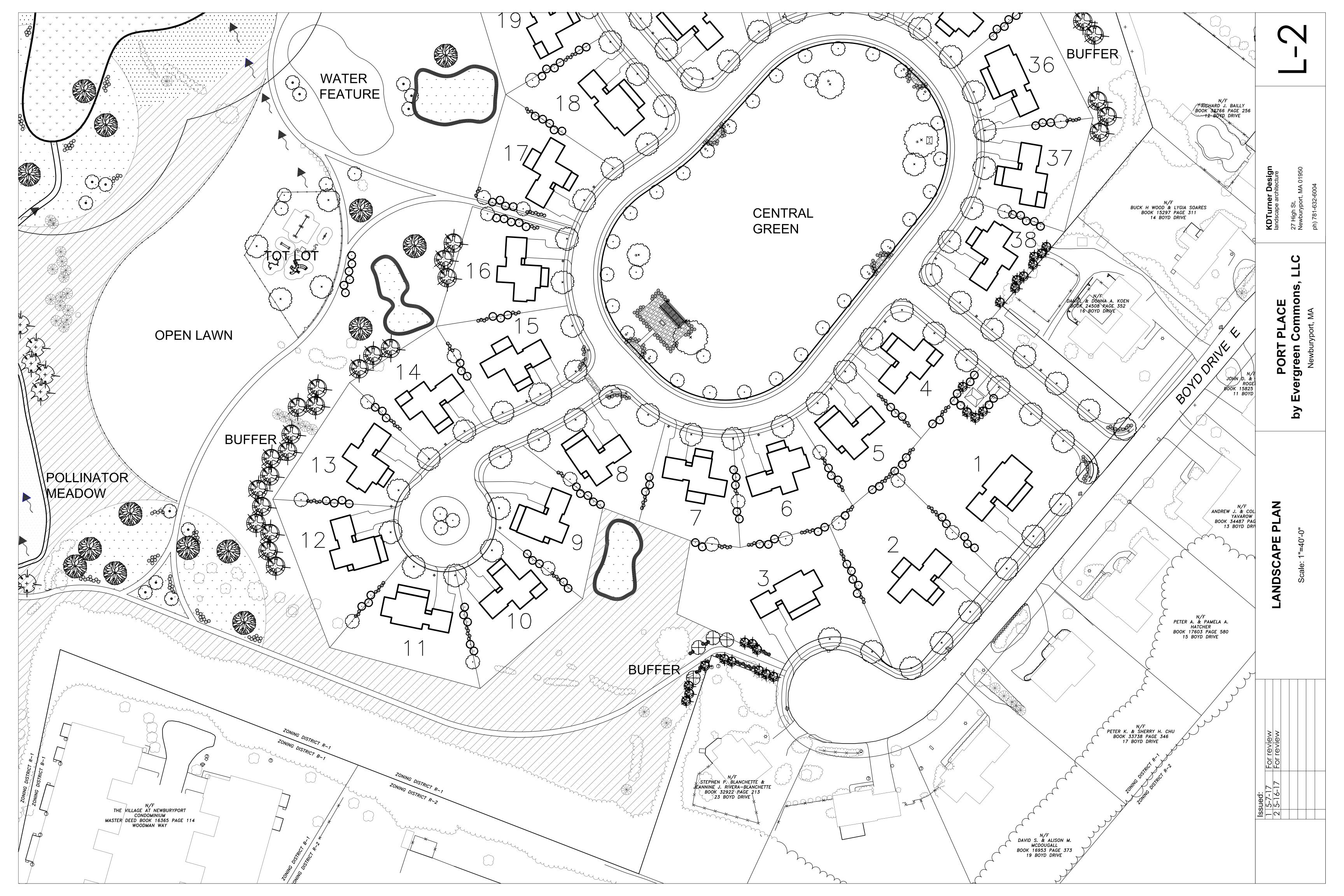
REVIEW COMMENTS

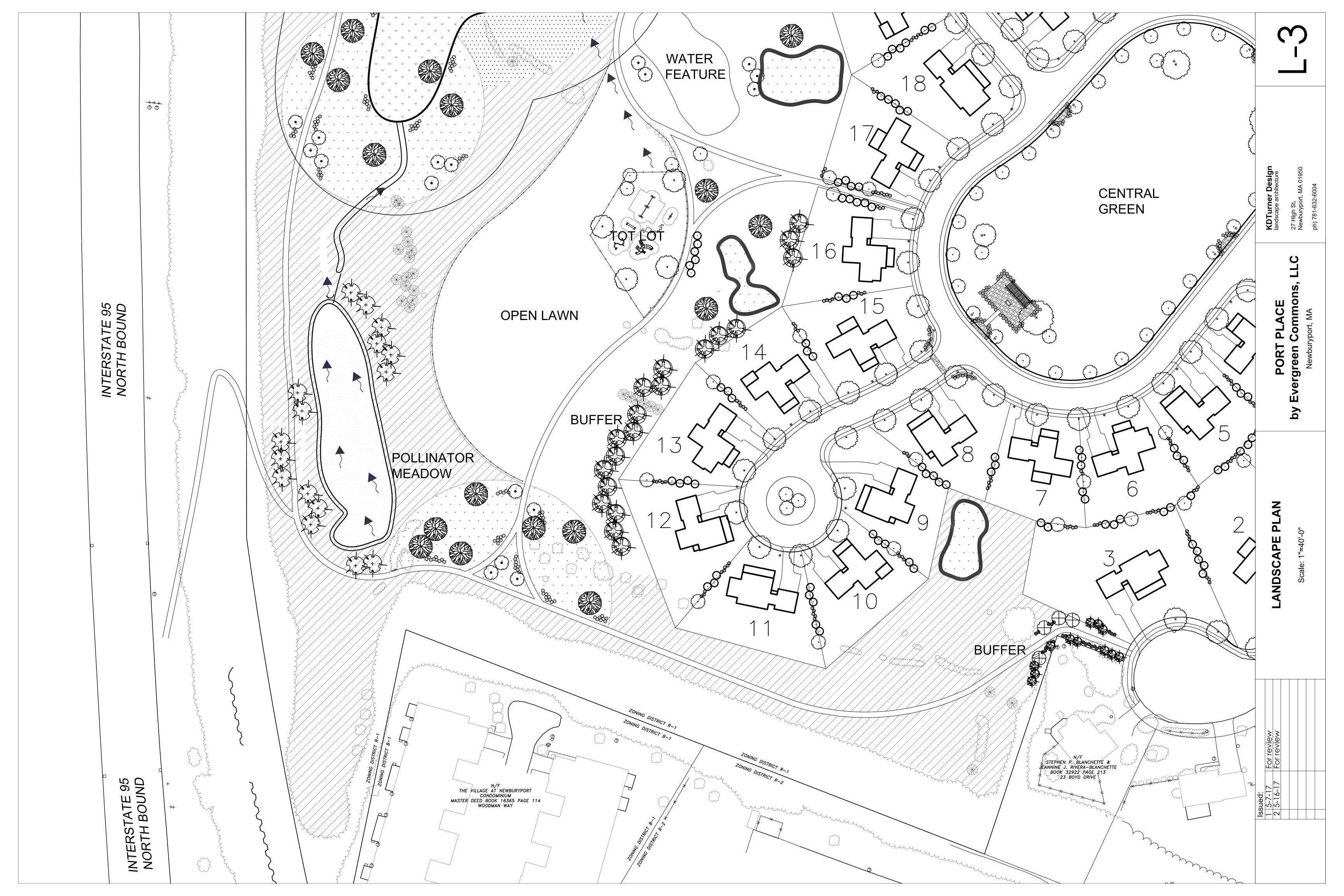
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TYPICAL **DETAILS &** NOTES 4



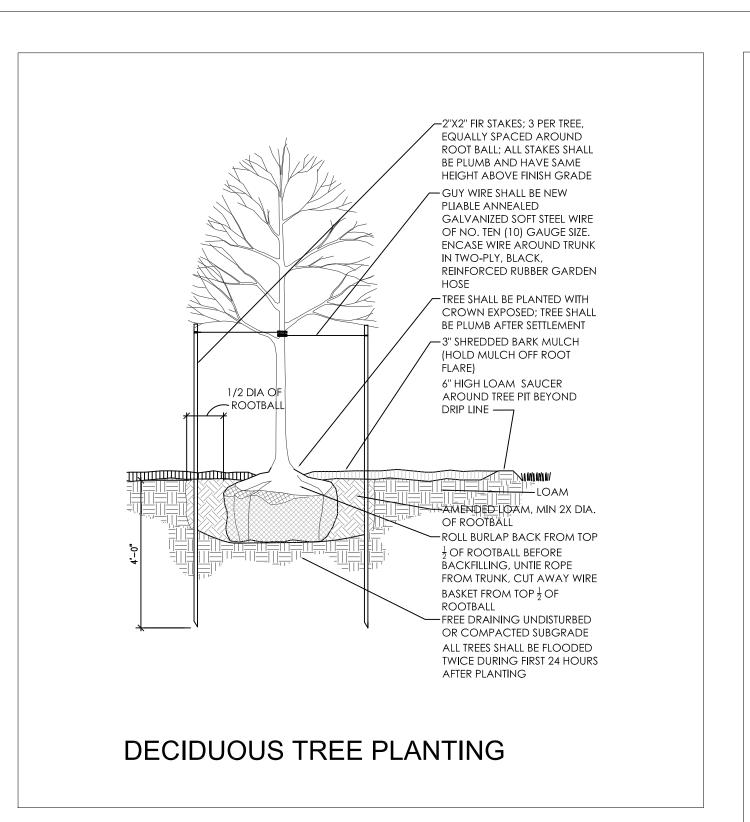


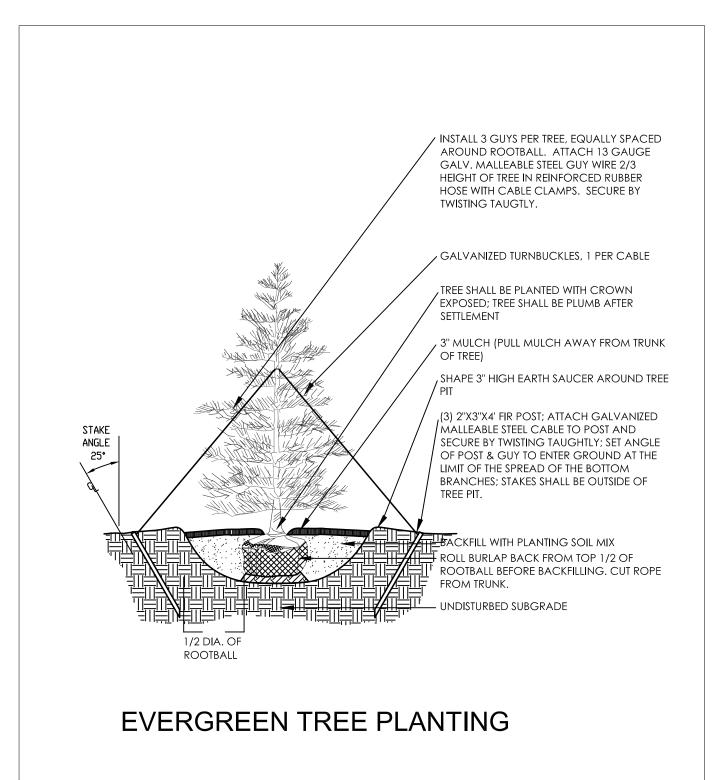


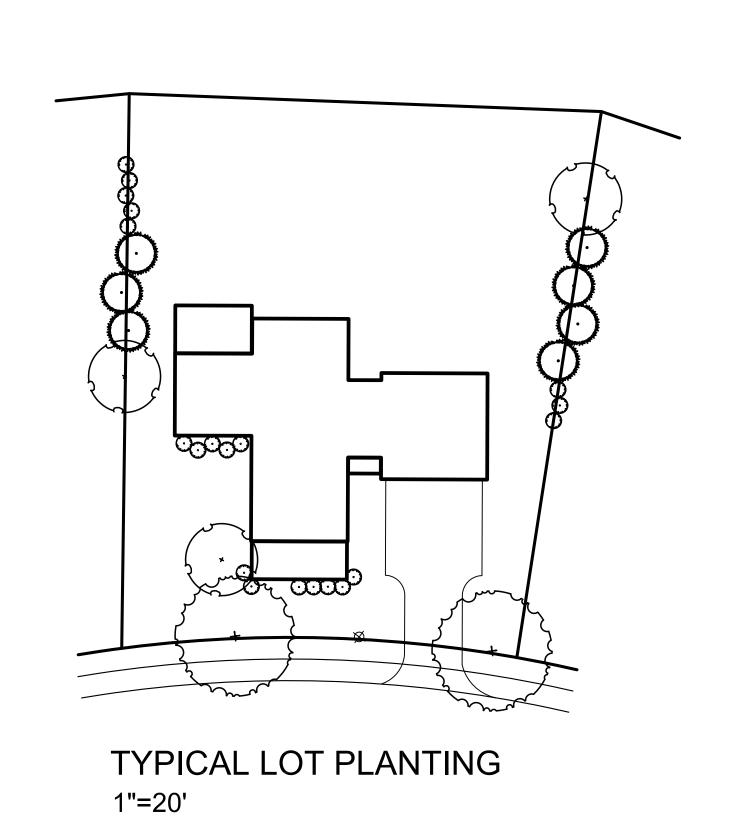












TRAIL TREE PLANTING

1"=40'

EVERGREEN BUFFER: QTY 7

JUNIPERUS 'MOONGLOW'JUNIPER6-8' HT.THUJA PLICATAARBORVITAE6-8' HT.

FLOWERING TREES: QTY 3

		0	
\	AMELANCHIER CANADENSIS	SERVICEBERRY	2" CAL.
ş	CORNUS FLORIDA	FLOWERING DOGWOOD	2" CAL.
,	PRUNUS X YEDOENSIS	YOSHINO CHERRY	2" CAL.

SHRUBS: QTY 20

SHRUBS: QTY ZU					
AZALEA 'DEL VALLEY WHITE'	AZALEA	3 GAL.			
HYDRANGEA 'END. SUMMER'	HYDRANGEA	3 GAL.			
ROSA 'KNOCKOUT'	ROSE	3 GAL.			
SPIREA 'SHIROBANA'	SPIREA	3 GAL.			
SYRINGA MEYERI 'PALABIN'	DWARF KOREAN LILAC	3 GAL.			

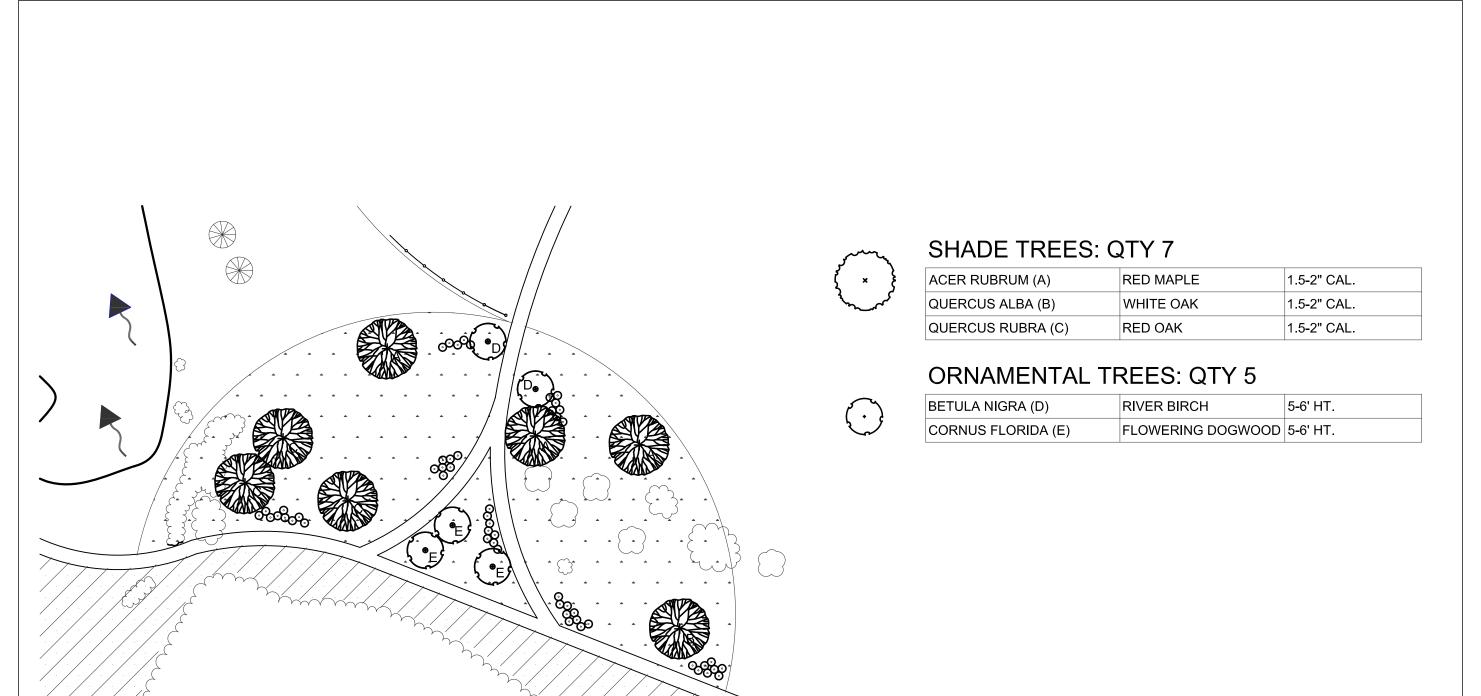
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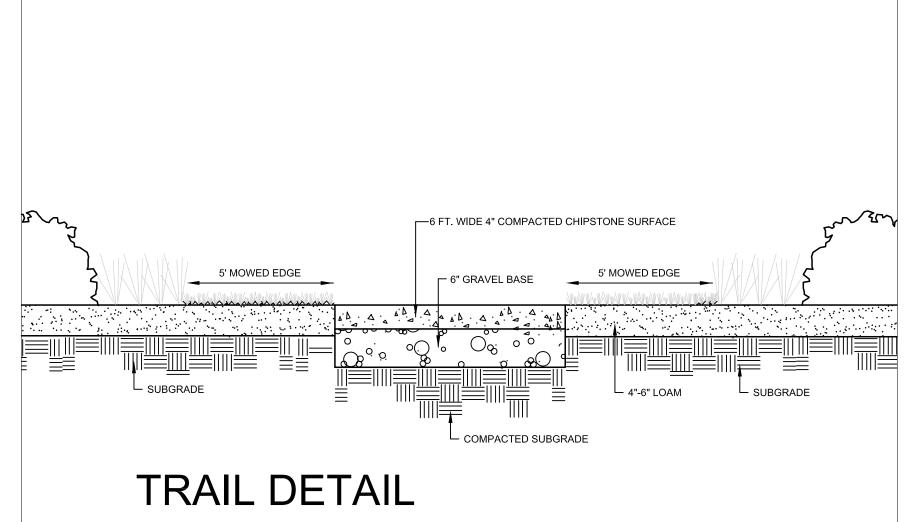
LIGHTING SCHEDULE

♦ STREET LIGHTS: QTY 34

UPLIGHTS: QTY 20

* SEE CIVIL PLAN FOR SPEC SHEETS









MODEL TYPE B FRONT ELEVATION



MODEL TYPE D FRONT ELEVATION



MODEL TYPE C FRONT ELEVATION



MODEL TYPE E FRONT ELEVATION WITH SUNROOM



MODEL TYPE F FRONT ELEVATION

Design Consultants Inc.
Somerville - Quincy - Newburyport
www.dci-ma.com FOR REGISTRY OF DEEDS USE ONLY

Winter GEC, LLC

34 WINTER STREET NEWBURYPORT, MA 01950 978-270-8626

PROJECT TEAM

18 BOYD DRIVE, SUBDIVISION NEWBURYPORT, MA

PREPARED FOR EVERGREEN COMMONS, LLC

PROJECT INFO

REV DESCRIPTION

ARCHITECTURAL **ELEVATIONS**

CITY OF NEWBURYPORT OFFICE OF THE TOWN CLERK NEWBURYPORT PLANNING BOARD SHEET NAME: I, CLERK OF THE CITY OF NEWBURYPORT,
HEREBY CERTIFY THAT THE NOTICE OF
APPROVAL OF THIS PLAN BY THE
PLANNING BOARD HAS BEEN RECEIVED
AND RECORDED AT THIS OFFICE AND NO
NOTICE OF APPEAL WAS RECEIVED
DURING THE TWENTY DAYS NEXT AFTER
SUCH RECEIPT AND RECORDING OF SAID
NOTICE.

DATE

CLERK

DR BY: WAK CHK BY: SBS PROJ NO: 2015-063 DATE: MAY 22, 2017 SCALE: N.T.S.